

Reference: EDC/24/0147

Site Address: Ashmere (Phase 3) Eastern Quarry, Watling Street, Swanscombe, Kent

Proposal: Reserved Matters application (relating to access, appearance, landscaping, layout and scale) pursuant to condition 2 (details of Reserved Matters), Condition 25 (Reserved Matters Design Details) and Condition 28 (Noise Report) of outline planning permission EDC/17/0048 for 216 dwellings and associated infrastructure including roads, surface water attenuation and electricity sub-station.

Applicant: Countryside Clarion (Eastern Quarry) LLP

Parish / Ward: Ebbsfleet

SUMMARY:

This application seeks approval of reserved matters in relation to access, appearance, scale, layout and landscaping for the erection of 216 dwellings and associated infrastructure representing the third phase of delivery in Ashmere village.

The application has been guided by a series of strategy documents and principally the Area Masterplan and Design Code (AMP/ADC) which set out design parameters for delivery of detailed consents and set the standards against which all reserved matters applications in the Western Village are assessed.

The proposal adheres to the AMP/ADC and sets out an extremely high quality of design with ten 'Greens' and two 'Ambers' in the Building for a Healthy Life categories. Design quality has been achieved through high quality architectural detailing appropriate to its setting and responds well to the design narratives set out in the AMP/ADC. A legible layout is identified which incorporates landscaped areas, impromptu play opportunities and legible and safe routes for cyclists and pedestrians. All units fully comply with the Nationally Described Space Standards and all dwellings meet Part M4(2) of the Building Regulations with the exception of the Flats Over garages (FOGs), which significantly exceeds the S106 planning requirement for this site.

The scheme is in general compliance with the Parking Management Plan, balancing car parking provision against high quality landscaping. Visitor spaces are well distributed within the site. It is considered the amount of parking is in the region of the required standards and any further provision would erode landscaping to the detriment of the landscape-led design approach.

The application delivers on Garden City principles, providing beautifully and imaginatively designed homes with gardens and high-quality affordable units, orientated towards public transport and which provides strong links to open spaces and surrounding green infrastructure, providing a safe and attractive living environment for future residents. Buildings have been designed to exceed the baseline sustainability levels under Part L of the Building Regulations and as such would meet the energy-positive technology objective.

The applicant has worked closely with the EDC to put forward a carefully considered scheme which complies with national and local policy and the objectives of the Ebbsfleet

Implementation Framework, and which will contribute to essential national housing growth and the development of the Ebbsfleet Garden City.

RECOMMENDATION: Authority be delegated to the Director of Planning and Place to approve the application and issue reserved matters approval subject to the imposition of the following planning conditions with authority delegated to the Director of Planning and Place to make minor changes to the wording.

CONDITIONS:

Time limit

1. The development hereby permitted shall be begun before the expiration of two years from the date of this reserved matters approval.

Reason: For the avoidance of doubt and to ensure the development is brought forward within a specified time scale.

Approved Plans and Documents

2. The development hereby permitted shall be carried out in accordance with the following plans:

Layout Drawings:

B100184-AD-5000 Rev G – Site Location Plan
B100184-AD-5100 Rev N - Proposed Site Layout (Ground Level)
B100184-AD-5101 Rev C – Proposed Typology Plan
B100184-AD-5107 Rev D – Proposed Roof Plan
B100184-AD-5102 Rev F – Proposed Tenure Plan
B100184-AD-5103 Rev C– Proposed Refuse Storage Plan
B100184-AD-5104 Rev C – Proposed Storey Heights Plan
B100184-AD-5105 Rev G – Proposed Parking Plan
B100184-AD-5106 Rev D – Proposed Density Plan

Floor Plans & Elevations - Houses:

B100184-AD-5300 Rev E – 2B4P HT1 Type 1
B100184-AD-5310 Rev D – 2B3P HT2 Type 1
B100184-AD-5311 Rev D – 2B3P HT2 Type 2 Semi-Detached
B100184-AD-5312 Rev D – 2B3P HT2 Type 2 Detached
B100184-AD-5320 Rev E – 3B5P HT3 Type 1 Detached
B100184-AD-5321 Rev E – 3B5P HT3 Type 1 Semi-Detached
B100184-AD-5322 Rev E – 3B5P HT3 Type 2
B100184-AD-5340 Rev F – 3B5P HT4 Type 1
B100184-AD-5341 Rev E – 3B5P HT4 Type 2
B100184-AD-5350 Rev D – 4B7P HT5 Type 1
B100184-AD-5351 Rev G – 4B7P HT5 Type 2
B100184-AD-5360 Rev F – 4B6P HT6 Type 1 Semi-Detached
B100184-AD-5361 Rev E – 4B6P HT6 Type 1 Detached
B100184-AD-5362 Rev E – 4B6P HT6 Type 2
B100184-AD-5363 Rev F – 4B6P HT6 Type 3
B100184-AD-5364 Rev E – 4B6P HT6 Type 4
B100184-AD-5365 Rev E – 4B6P HT6 Type 5
B100184-AD-5366 Rev E – 4B6P HT6 Type 6
B100184-AD-5367 Rev F – 4B6P HT6 Type 7

Floor Plans & Elevations - FOGs:

B100184 AD-5260 Rev D – FOG A Type 1 Plans
B100184 AD-5261 Rev D – FOG A T1 Elevations
B100184 AD-5265 Rev D – FOG A T2 Plans
B100184 AD-5266 Rev E – FOG A T2 Elevations
B100184 AD-5250 Rev C – FOG B Floor Plans & Elevations
B100184 AD-5270 Rev E – FOG C Floor Plans & Elevations

Floor Plans & Elevations - Apartments:

B100184-AD-SK300-01 – Entrance Canopy width comparison
B100184 AD-5200 Rev G - Building A Ground Floor Plan
B100184 AD-5201 Rev F – Building A First and Second Floor Plan
B100184 AD-5202 Rev F – Building A Third Floor and Roof Plan
B100184 AD-5203 Rev F – Building A Elevations
B100184 AD-5210 Rev G – Building B Ground Floor Plan
B100184 AD-5211 Rev D – Building B First and Second Floor Plan
B100184 AD-5212 Rev E – Building B Third Floor and Roof Plan
B100184 AD-5213 Rev F – Building B Elevations
B100184 AD-5220 Rev F – Building C Ground Floor Plan
B100184 AD-5221 Rev D – Building C First and Second Floor Plan
B100184 AD-5222 Rev E – Building C Third Floor and Roof Plan
B100184 AD-5223 Rev D – Building C Elevations
B100184 AD-5230 Rev G – Building D Ground Floor Plan
B100184 AD-5231 Rev D – Building D First and Second Floor Plan
B100184 AD-5232 Rev D – Building D Third and Roof Plan
B100184 AD-5233 Rev E – Building D Elevations

Floor Plans & Elevations - Garages:

B100184-5280 - Double Garages Floor Plans
B100184-5281 - Double Garages Elevations
B100184/5283 - Garage Floor Plans and Elevations

Street Scenes:

B100184-AD-5400 Rev I - Site Sections/Street Scenes (Sheet A)
B100184-AD-5401 Rev F - Site Sections/Street Scenes (Sheet B)

Technical:

173534-010 Rev J – Levels Strategy
173534-090 Rev G – Phase 3 Highways Layout
173534-110 – Refuse and Servicing Swept Path Analysis
173534-120 - Car Parking Swept Path Analysis
173534-130 - Parking Restrictions Plan
173534-020 Rev H - Drainage Strategy Sheet 1
173534-020 Rev H - Drainage Strategy Sheet 2 (Phase 3 aspect only)

Landscape:

Landscape Commentary (Pegasus Group, ref P21-2399, dated January 2025)
P21-2399_EN_0006_S1_Rev C – Block C Play on the Way Landscape Proposals
P21-2399_EN_0007_S1_Rev A – Blocks A & D Play on the Way Landscape Proposals
P21-2399_EN_0004_S1_Rev C – Ecological Enhancements
Landscape Management Plan (Pegasus, Ref: P21-2300 R0001 V4, dated February 2025)
P21-2399_EN_0001_S1_Rev D – Illustrative Landscape Masterplan

Lighting:

Outdoor Lighting Reports – Secondary Roads – R3 (MMA, dated 17 September 2024)
Outdoor Lighting Reports – Tertiary Roads – R3 (MMA, dated 17 September 2024)
EIA and Lighting Design – s38 Lighting Design (MMA, Rev R3, dated 17/09/2024)
Proposed Lighting – Equipment Schedule – R3 (MMA, dated 16 September 2024)
Proposed Lighting - Equipment Schedule – R3 (MMA, dated 17 September 2024)
MMA17646/001 Rev R3 - S38 Street Lighting Design (Sheet 1 of 1)
Proposed Lighting – Risk Assessment – R3 (MMA, dated 16/09/2024)
Proposed Lighting – Risk Assessment – R3 (MMA, dated 17/09/2024)
EIA and Lighting Design – Private Lighting Design (MMA, Rev R3, Dated 16/09/2024)
Outdoor Lighting Report – Private Roads - R3 (MMA, ref: MMA17647, dated 16 September 2024)
MMA1764/001 Rev R3 – Private Street Lighting Design (Sheet 1 of 1)

Others:

Stage 1 RSA – Designers Response (Arden, Ref: 173534-06, dated October 2024)
B100184-AD Rev D - Ashmere Parcel 9 – Accommodation Schedule (dated 11.02.2025)
B100184-AD-5370 Rev B – Sub-station Floor Plans and Elevations
173534-100 Phase 3 Existing Levels Plan

Reason: For avoidance of doubt and to ensure a satisfactory form of development.

Prior to the installation of underground services

3. **Services and Trees** - (A) Notwithstanding the details shown on the approved plans, no installation of underground services shall be carried out until a detailed plan of underground services showing the relationship with trees within streets and open spaces, together with details of the final layout and spacing of trees in relation to street lighting, have been submitted to and approved in writing by the Local Planning Authority. The submitted details shall demonstrate that no conflicts will arise between underground utilities and tree roots. Details and locations of root barriers and all below ground root cells shall be submitted clearly demonstrating that trees and cells have access to sufficient soil volumes and that trees have sufficient room to establish. Details of permeable paving surrounding the planters and the tree planting or details showing positive drainage into the root cells shall also be provided. The development shall be carried out in accordance with the approved details.

(B) Prior to the installation of hard landscaping, photographic evidence of below ground tree root cells being implemented in accordance with part A above shall be submitted to and approved in writing by the Local Planning Authority to ensure that the Local Planning Authority are satisfied with the installation of the tree root cells.

Reason: To ensure a satisfactory landscape quality and appearance within the development and to ensure that underground servicing will not result in conflict with landscaping design and trees will establish with sufficient access to water and oxygen to flourish in the interests of neighbouring amenity, in accordance with Dartford Local Plan 2024 Policies M1 and M14.

Prior to development above foundation level

4. **Architectural Detailing** - Notwithstanding the details submitted with the application, no commencement above foundation level shall take place until details and samples of the materials (where appropriate) for the house types, apartments and substation for the following elements have been submitted to and approved in writing by the Local Planning Authority:

- a) Architectural detailing (at scale 1:20) including:
- i. Details of each roof envelope including roof edges, parapets, copings, and rooftop plant screening
 - ii. Key junctions/bonds between materials/finishes, including the elevational location of all joints required for movement, and lead flashing
 - iii. Details of all openings, including head, jamb, and sill details, including profiles, for typical openings and all ground floor entrances and doors to balconies / terraces demonstrating level accessible thresholds
 - iv. Principal features on the facades
 - v. Balconies and balustrades including: plans, sections and elevations detailing structure, soffits, kickboards, downpipe drainage, balustrading, floor finishes and terraces (to ground floor balconies), demonstrating level accessible thresholds.
 - vi. Elevational location and details of all items which are fixed to/incorporated within the façades including, air vents, ventilation grills, flues and/or louvres extraction systems (e.g. ASHP), rainwater pipes, bird & bat boxes, lighting, dry-riser inlet box, apartment signage, electricity meter cupboards, alarms including any provision for cable runs boxes.
 - vii. External rainwater goods
 - viii. External lighting (in relation to design/appearance)
 - ix. All entrance canopies and entrance porches including fixing and soffit details
 - x. Building name signage for the four apartment blocks

The development shall be carried out in accordance with the approved details.

Reason: To ensure that the appearance of the development is satisfactory and without prejudice to conditions of visual amenity in the locality, in accordance with the Dartford Plan 2024 Policy M1.

Informative: The materials to be used on the inside of car ports should match the external walls.

5. **External Materials** - Notwithstanding the details set out in the approved plans, no development above foundation level shall take place until details of materials and products, including finishes, of external materials to be used on the buildings (houses, apartments, and substation) hereby approved have been submitted to and approved in writing by the Local Planning Authority. These shall include (but are not limited to):

- a. All façade and roof cladding materials
- b. All brick and mortar types including brick bond, mortar colour and joint profile, copings and parapet capping, including feature brickwork elements (as listed under planning condition 3)
- c. Fenestration including window / door types (including finishes, glass types and any manifestation, depth of reveals), window cills and headers, spandrel panels, frames for the approved house types and apartment buildings

- d. Facing metalwork (including balcony balustrades, service doors, screens, gates)
- e. All items which are fixed / integrated to the façade (e.g. fins/louvres, vent grilles, rainwater pipes, bird/bat boxes, bee brick, canopies)
- f. Soffits and canopy materials
- g. Balcony and terrace floor finishes

Samples of the above materials should be provided. Sample panels of facades shall be provided with details and sizes to be agreed with the Local Planning Authority in advance. The development shall be carried out in accordance with the approved details.

Reason: To secure high-quality design and detailing and to ensure that materials will make an acceptable contextual response, resulting in the satisfactory appearance of the development in accordance with the Dartford Plan 2024 Policy M1.

Informative: Ground floor apartment doors onto terraces must have an external lock to ensure its functionality as an off-street entrance.

6. **Boundary Treatments** - Notwithstanding drawing P21-2399_EN_0002_S1_REV C (Boundary Treatments), no development shall take place above foundation level until a revised plan has been submitted to and approved in writing by the Local Planning Authority that:

- (a) extends the low profile brick boundary walls with railings detailed on the drawing along the front boundaries of the houses fronting Fastrack and Ashmere Village Green; and
- (b) replaces the hedge to the southern side of the parking courtyards for Blocks A and D with hit-and-miss brick wall as detailed on the drawing.

The development shall be carried out in accordance with the approved details prior to first occupation of the building to which they relate and shall be retained and maintained thereafter.

Reason: To accord with the requirements of the approved Area Master Plan and ensure a satisfactory appearance to the development in the interests of visual amenity within the area in accordance with the Dartford Plan 2024 Policy M1.

7. **Soft Landscaping** - Notwithstanding the details submitted with the application, no development above foundation level shall take place until the following landscape details have been submitted to and approved in writing by the local planning authority:

- a. Detailed planting plans at 1:200 scale for the whole scheme including details of the numbers of all trees, hedges and shrubs to be planted, together with a planting schedule providing pot sizes, heights and spread, densities and total quantities of individual species
- b. Details of the small, planted beds to the north of Plots 576-584
- c. Details of climbing plant supports

Tree species and sizes to be informed by EDC's Public Realm Strategy guidance. The approved planting scheme shall be carried out within the first planting season following the completion of the development, or in accordance with a timetable agreed in writing by the Local Planning Authority, and shall be managed in accordance

with the Landscape Management Plan (Pegasus, Ref: P21-2300 R0001 V4, dated February 2025) at all times.

Reason: To ensure a satisfactory appearance to the development; there are no conflicts between services and street furniture that would result in conflict with landscaping design and to ensure that the plants and climbers will have sufficient soil available to reach maturity and achieve the desired level of greening, in accordance with the Dartford Plan 2024 Policies M14 and E2.

Informative 1: The carriageway of the Secondary Road has included the widened area on the western side of the south-western bend and decreased the width of the highway verge. As agreed with the applicant during discussion, it will be required that within the adjacent Phase 4 development that the outside (western) verge be widened on the Phase 4 side of the site boundary and the trees moved back within the verge to prevent damage by large vehicles.

Informative 2: The applicant is reminded that condition 65 of the outline permission requires that landscaping schemes approved as reserved matters applications 'shall be implemented during the first planting season following completion of the relevant area of the development and shall thereafter be maintained for a period of five years. Any trees, shrubs or grassed areas which die, are diseased or vandalised within this period shall be replaced within the next planting season.'

8. **Hard Landscaping** - Notwithstanding the details set out in the approved plans, no development above foundation level shall take place until details and samples of proposed hard surfacing materials have been submitted to and approved in writing by the Local Planning Authority. These details shall substantially accord with the details illustrated and annotated on the approved drawings and include the following details:

- a. Full details of all hard surfacing materials to be used in the external finishes of all roads, footpaths, courtyards, parking areas, terrace areas for ground floor apartments including the size, material, laying pattern and colour finish of all hard surfaces
- b. Details of feature paving areas
- c. Specification of edging and kerbs across the site, in accordance with the Eastern Quarry Area Masterplan/Design Codes requiring the use of conservation kerbs
- d. Details of materials, separating kerbing and edge restrains, to be used for the overrun area on the eastern side of the south-western corner of the secondary road (see Informative)
- e. Details of inclusive design including external steps and ramps, tactile warning or wayfinding paving, mobility features and dropped kerbs
- f. Details of the demarcation of vehicle parking spaces within parking courts @1:100 or greater scale
- g. Details of the demarcation of on-site parking spaces @1:100 or greater scale
- h. Details of how the secondary cycle route is differentiated from the primary cycle route in its construction/finish/markings @1:100 or greater scale
- i. Details of the location and cycle stand design and numbers for short-stay cycle parking at the four apartment blocks
- j. Full specification details for all street furniture including details of seating, cycle stands, bollards, bins, pergolas, cycle stands, community and raised planters (not exhaustive) in accordance with the EDC Public Realm Strategy guidance
- k. Details of water butts to be provided to all private residential gardens

The development shall be carried out in accordance with the approved details and the approved hard landscaping shall be completed prior to the final occupation of the development and thereafter maintained in accordance with the approved Landscape Management Plan (Pegasus, Ref: P21-2300 R0001 V4, dated February 2025) at all times.

Reason: In order to ensure an acceptable and functional standard of development upon completion, and in the interests of visual amenity, in accordance with the Dartford Plan 2024 Policies M14 and E2.

Informative: The carriageway of the Secondary Road, including the widened area on the western side of the south-western bend should be constructed in black asphalt. The overrun area on the eastern side of the south-western bend should be designed such that it does not encourage general use, which may enable vehicles to travel faster through widening the bend, but enables use when necessary. Any materials, and the associated structural design of this area, including any kerbing separating the overrun from the main carriageway, must be sufficient to withstand repeated HGV overrun. The use of block paving with suitable detailing and appropriate edge restraints is likely to provide the desired effect, with expected reduced maintenance requirements compared to "cobble" setts or similar products.

9. **Powder-Coated Lighting Infrastructure** - Notwithstanding the submitted lighting details, no construction above foundation level shall take place until details of the design and finish of powder-coated light columns and illuminated bollards to be erected both in areas to be adopted as public highway and retained in private ownership have been submitted to and agreed in writing with the Local Planning Authority. The development shall be carried out in accordance with the approved details and implemented prior to first use of the area to which they relate and shall thereafter be retained at all times.

Reason : To ensure a satisfactory appearance to the development in accordance with the Dartford Plan 2024 Policy M1.

10. **Parking Court Lighting** - Notwithstanding the submitted lighting details, no construction above foundation level shall take place until details of lighting to the car parking courts have been submitted to and agreed in writing with the Local Planning Authority. Details should demonstrate that lighting within all car parking courts will be in line with table 4 of BS5489-1:2020 'Design of road lighting - Lighting of roads and public amenity areas' in terms of 'light traffic' for 'parking areas of terraced and apartment houses' (e.g. average 5 lux and uniformity of 0.25). The development shall be carried out in accordance with the approved details and implemented prior to first use of the area to which they relate and shall thereafter be retained at all times.

Reason: To ensure a satisfactory appearance to the development and secure an appropriate level of lighting which will conserve and enhance the amenity of residents, neighbours, and wildlife habitats and public safety in the interests of secure by design and in accordance with Dartford Local Plan Policies M1 and M2.

11. **Play-on-the-way** - Notwithstanding the details set out in the approved plans, no development above ground floor slab level shall take place until details for each 'Play on the Way' space, including all supplier details and product information for play equipment/features, play experience expectations, and precedent images of all play kit, in combination with all related hard and soft landscaped surface, boundary treatments and a timescale for the provision of each space has been submitted to and approved in writing by the Local Planning Authority. The approved play

equipment/features, as well as required ancillary and landscaping works, shall be installed in accordance with the approved details and timescale and shall thereafter be maintained in accordance with the Landscape Management Plan (Pegasus, Ref: P21-2300 R0001 V4, dated February 2025) at all times.

Reason: In order to ensure an acceptable and functional standard of development upon completion, and in the interests of visual amenity and effective and experiential landscaping in accordance with the Dartford Plan 2024 Policies M1 and M13.

12. **Car Club Vehicles** - Notwithstanding the 'Residential Travel Plan' 173534-03B Rev C (Arden, December 2024), no development above foundation level shall take place until car club details have been submitted to and approved in writing by the Local Planning Authority. The details shall include:

- a. the permanent location of 2no. car club parking spaces within the boundary of the application site, including associated electric vehicle charging provisions;
- b. the operational arrangements for the car club;
- c. mechanism for advertising the agreed with one year's free membership to the on-site car club and the availability of £50 free driving credit;
- d. prior to the first occupation of any of the units hereby approved a minimum of 1no. electric car club vehicle shall be made available for public use from the approved spaces within the phase and shall be retained and maintained for it/their designated purpose(s) at all times. prior to the first occupation of the 100th unit within the phase the second electric car club vehicle shall be made available for public use from the approved spaces within the phase and shall be retained and maintained for their designated purpose(s) at all times.

Reason: In the interests sustainable travel, in accordance with the Dartford Plan 2024 Policies M15, M16 and E2.

13. **Green Roofs** - No development above foundation level shall take place until full details of the green roofs shown indicatively on the approved roof plan drawings for the four apartment blocks hereby approved have been submitted to and approved in writing by the Local Planning Authority. Details shall include the size, location, appearance, form and technical specification of the green roof, details of green roof planting, and arrangements for management and maintenance across the lifetime of the development. The green roofs shall be installed in accordance with the approved details prior to first use of the building to which they relate and shall thereafter be maintained and retained at all times.

Reason: To ensure compliance with the approved Biodiversity Action Plan for Eastern Quarry upon completion and implementation of on-site and biodiversity enhancements, in accordance with the Dartford Plan 2024 Policy M14.

Informative : The Kent County Council Ecologist recommends that native sedum varieties are included on in the species mix of the green roofs to benefit biodiversity.

14. **ASHP & MVHR Details (Houses and FOGs - Appearance)** - No development above foundation level shall take place until final details of the location, appearance, and form of all Air Source Heat Pumps (ASHP) and Mechanical Ventilation Heat Recovery (MVHR) units to be installed within all houses and FOGs hereby approved, have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and the ASHP and MHVR units shall be installed prior to first occupation of the dwelling to which they relate and shall thereafter be retained in working order at all times.

Reason: To ensure the sustainable development of the site in accordance with the aims of the Dartford Plan 2024 Policy M3 and to protect the appearance of the site in accordance with the Dartford Plan 2024 Policy M1.

Informative: No ASHP or other mechanical units will be agreed to the front of dwellings or the side if they would then be visible from the public realm.

15. **Electric Cycle Parking Details (Apartments)** - No construction above foundation level shall take place until details of the provision of charging facilities for electric cycles within communal residential cycle stores within the four apartment buildings hereby permitted have been submitted to and approved in writing by the Local Planning Authority.

Reason: In order to encourage sustainable travel, in accordance with the Dartford Plan 2024 Policies M15 and M16.

16. **Cycle Storage** - No construction above foundation level shall take place until full details of the residential cycle storage facilities for the houses hereby approved, have been submitted to and approved in writing by the Local Planning Authority. The details shall include the design, size and location of storage facilities and shall demonstrate compliance with the Ebbsfleet Development Corporation Sustainable Travel Strategy requirements for cycle storage, including:

- A storage capacity to accommodate a minimum of 1 adult bicycle per bedroom within a dwelling
- Doors which are secured by mortice locks and not padlocks.
- Shed cycle storage comprising tongue and groove construction, with minimum 18mm thickness, securely fixed to a concrete foundation and ground anchors provided.
- At least 1m² storage space must be provided within the shed, for garden equipment etc, in addition to that provided for cycle parking.
- where more than two bicycle spaces are required, the scheme should include either a 'Sheffield' stand or wall bars to provide a secure fixing point.

The approved cycle storage facilities shall be constructed, furnished, and made available for use prior to the relevant dwelling being first occupied. The cycle storage facilities shall thereafter be retained and kept available for storage of cycles by the residents at all times.

Reason: In order to encourage sustainable travel, in accordance with the Dartford Plan 2024 Policies M15, M16 and E2.

17. **EV Car Charging** - No development above foundation level shall take place until details of the proposed active electric vehicle charging points (comprising SMART sockets connected to the electrical supply system that vehicle owners can plug their vehicle into, with a minimum output rating of 7kW- Mode 3, AC) and passive electric vehicle charging provision (comprising provision of the network of cables and an associated power supply necessary to connect to future EV charging points), as detailed within the Highways Technical Note Addendum (173534-07 December 2024), have been submitted to and approved in writing by the Local Planning Authority. The details shall include confirmation of arrangements for management and maintenance of the electric vehicle charging points that are located within communal car parking areas / privately maintained highway verges. The approved active and passive electric vehicle charging points shall thereafter be installed prior to first occupation of the relevant dwelling, in the case of EV serving allocated parking spaces, or the first

occupation of any dwellings within the relevant street, in the case of EV serving visitor parking spaces. The facilities shall be maintained in a good working order at all times.

Reason: In the interests of good design and sustainable travel, in accordance with Dartford Plan 2024 Policies M15, M16, M3 and E2.

Prior to First Occupation

18. Vehicle Parking - The dwellings hereby approved shall not be occupied until the parking spaces associated with the relevant dwelling(s) as shown on the Proposed Parking Plan (drawing number B100184-AD-5105 Revision G) as vehicle parking spaces /garages/ car ports for the relevant dwelling(s) has/have been provided, including final surfacing, drainage and demarcation. The spaces identified as parking for visitors as shown on the Proposed Parking Plan (drawing number B100184-AD-5105 Revision G) shall be provided, including final surfacing, drainage and demarcation prior to the occupation of the first dwellings within the street in which they are located. Visitor parking spaces shall be retained as unallocated parking. Thereafter, no permanent development, whether or not permitted by the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order amending, revoking or re-enacting that Order) shall be carried out on the area of land reserved for vehicle parking or in such a position as to preclude its use for vehicle parking.

Reason: In the interests of highway safety in accordance with the Dartford Plan 2024 Policies M15 and M16.

19. Cycle Parking (Apartments) - No apartments hereby approved shall be occupied until the cycle storage facilities for the relevant apartment building, including the approved charging facilities for electric cycles, demonstrating circulation and access within communal cycle stores, lighting of cycle stores, the provision of spaces for adapted bikes and a minimum size to accommodate 1 adult bicycle per bedroom, in accordance with the Ebbsfleet Development Corporation Sustainable Travel Strategy, have been constructed, furnished and made available for use. The cycle storage facilities shall thereafter be retained and kept available for use by the residents at all times.

Reason: In order to encourage sustainable travel, in accordance with the Dartford Plan 2024 Policies M15, M16 and E2.

20. Visitor Cycle Parking - No apartments hereby approved shall be occupied until details of the external visitor cycle parking facilities for the development have been submitted to and approved in writing by the Local Planning Authority. No apartments shall be occupied within an apartment building until the approved visitor cycle parking facilities to serve the relevant apartment building have been installed and made available for use. The cycle storage facilities shall thereafter be retained and kept available for use by the residents at all times.

Reason: In order to encourage sustainable travel, in accordance with the Dartford Plan 2024 Policies M15, M16 and E2.

21. Visibility Splays (Vehicle) - The dwellings hereby approved shall not be occupied until the visibility splays as set out in the Phase 3 Highway Layout (drawing number 173534-090 Revision G), have been provided. The splays shall be kept clear of

obstructions over 0.6 metres above carriageway level within the splays and maintained as such at all times.

Reason: In the interests of highway and pedestrian safety, in accordance with the Dartford Plan 2024 Policies M15 and M16.

22. **Visibility Splays (Pedestrian)** - The dwellings hereby approved shall not be occupied until the pedestrian visibility splays shown on the Phase 3 Highway Layout (drawing number 173534-090 Revision G), have been provided. The splays shall be kept clear of obstructions over 0.6 metres above carriageway level within the splays and shall be maintained as such at all times.

Reason: In the interests of highway and pedestrian safety, in accordance with the Dartford Plan 2024 Policies M15 and M16.

23. **Car Park Management Plan** - Notwithstanding the submitted Car Park Management Plan, no residential unit hereby approved shall be first occupied until a full Car Parking Management Plan, with the aim of achieving appropriate use of the car parking areas and the prevention of inappropriate parking across the site has been submitted to and approved in writing by the Local Planning Authority. The Car Parking Management Plan shall include, but not be limited to:

- a. Details of how parking within formal spaces (residential and visitors) will be managed and enforced
- b. Details of how inappropriate parking across the site, informal parking outside of allocated parking spaces and exceedance of any time limits by individual vehicles will be managed and enforced
- c. Details of arrangements for the management and maintenance of electric vehicle charging points that are not on-plot
- d. Details for the management and maintenance of the car club vehicles/parking spaces
- e. Details for the management and maintenance of the sub-station parking space
- f. A programme of implementation, monitoring and management of the car park to be reviewed regularly and as necessary for the lifetime of the development.

The development shall thereafter accord with the latest approved CPMP at all times.

Reason: To ensure adequate management arrangements for residential and visitor car parking, the free flow of traffic on the highway, protect residential amenity and to encourage sustainable travel in accordance with the Dartford Plan 2024 Policies M15 and M16.

Informative 1 : The cessation of on-street parking enforcement at 18:00 daily is considered too early as to discourage the abuse of visitor parking spaces and informal parking by residents and their visitors, to the detriment of other residents' amenity and highway safety, and will not be acceptable.

Informative 2 : Kent County Council Highways and Transportation officers note that parking management should be in line with the rest of the Eastern Quarry site, adopted streets should be subject to parking restrictions secured by a TRO in order to prevent on street parking outside of designated bays and support the low parking strategy. Private streets should have private parking enforcement in perpetuity and the cost of the TROs shall be provided born by the Applicant.

24. Residential Travel Plan - Notwithstanding submission of the 'Residential Travel Plan' 173534-03B Rev C (Arden, December 2024), the dwellings hereby approved shall not be occupied until a Full Residential Travel Plan, to reduce dependency on the private car, has been submitted to and approved in writing by the Local Planning Authority. The Full Travel Plan shall include objectives and modal-split targets, a programme of implementation and provision for monitoring, review and improvement. Thereafter, the approved Travel Plan shall be implemented in full and adhered to throughout the life of the development, with monitoring taking place for a minimum of five years from first occupation.

Reason: In the interests sustainable travel, in accordance with the Dartford Plan 2024 Policies M15, M16 and E2.

25. Road Noise Mitigation - No occupation of any relevant dwelling hereby approved shall take place until all habitable rooms (bedrooms, living rooms) within the relevant dwelling have been constructed to incorporate glazing and ventilation units that, in accordance with Tables 4-2 and 4-3 of the Noise Assessment Addendum (Arden, Ref: 173534-R09A, dated January 2025), provide the following minimum sound reduction performance specifications:

- Glazing = 31 dB Rw
- Passive Ventilators = 35 dB Dn,e,w

The mitigation measures shall be maintained as such at all times.

Reason: To attenuate road traffic noise and provide suitable internal residential amenity in accordance with the Dartford Plan 2024 Policy M2.

26. Ecological Enhancement - No building hereby approved shall be occupied until the ecological enhancement measures, shown on drawing P21-2399_EN_0004_S1_REV C, related to that building and the sub-phase in which the building is situated, have been installed in accordance with approved details and the measures shall thereafter be maintained and retained at all times.

Reason: To ensure delivery of ecological and biodiversity mitigation and enhancement in accordance with the Dartford Plan 2024 Policy M14.

Informative 1 : The ecological enhancement plan details: 13 Swift bricks; 3 black redstart bird boxes; 16 integrated bat boxes; 10 integrated bee bricks will be installed within buildings in the site and hedgehog highways in all close board fencing. That plan shows the indicative position of the features. The KCC Ecologist advises that prior to any works commencing on site the applicant's ecologist annotates the construction plans to ensure it is clear exactly where the features will be located. This will ensure that they are incorporated in to the buildings during construction.

Informative 2 : The location of bat and bird boxes should be adjacent to open spaces, away from busier roads and lighting. Bird and bats have different environmental requirements so should not be found next to each other - birds prefer largely north facing and bats prefer south to east facing. The applicant should therefore review final bat and bird roost features with an ecologist to ensure optimum locations for bird and bat roost features are secured.

27. Sustainable Development - No development above foundation level shall take place until a final Energy and Sustainability Statement as a result of detailed design has

been submitted to and approved in writing by the Local Planning Authority. The Energy and Sustainability Statement must demonstrate that the scheme results in the reduction in carbon emission at 66.8% as detailed in the AES Sustainability Consultant's 'Energy and Sustainability Statement' (September 2024). If there is non-compliance with this, additional measures should be proposed, details of which shall be submitted to and approved in writing by the Local Planning Authority. No residential dwelling shall be occupied until the finally agreed measures for the relevant dwelling have been implemented in full and the finally agreed measures shall thereafter be retained and maintained in good working order throughout the lifetime of the development.

Reason: To ensure the sustainable development of the site, in accordance with the Dartford Plan 2024 Policy M3.

28. **Mechanical Plant Noise** - No development above ground foundation level shall take place until final details of the cumulative mechanical plant, including the location of the plant within each plot and the final details of the equipment, have been submitted to and approved in writing with the Local Planning Authority. The plant must demonstrate compliance with the cumulative noise, operating at maximum capacity, as per the Noise Assessment Addendum (Ardent, Ref: 173534-R09A, dated January 2025), in which the resultant noise would not exceed the upper CIEH/loA assessment criterion of 40 dB LAeq,15min at 1m from any noise sensitive façade of neighbouring residential premises. No residential dwelling shall be occupied until the finally approved measures for the relevant dwelling have been implemented in full and the finally approved measures shall thereafter be retained and maintained in good working order throughout the lifetime of the development.

Reason: The Noise Impact Assessment for cumulative plant was based on an illustrative scenario to achieve acceptable results in respect to a quiet living condition for future residents. The final equipment should not create a worse noise environment than the illustrated scenario, in order to protect residential amenity of the future occupants in accordance with the Dartford Plan 2024 Policy M2.

29. **Water Efficiency** - No dwelling shall be occupied until the water efficiency measures to achieve the water usage of no greater than 105 litres per person per day as identified in Section 10 of the Energy and Sustainability Statement Sept 2024 (AES, Rev 5 dated 10.01.2025) have been installed and thereafter such measures shall be retained for the lifetime of the development.

Reason: In the interests of sustainability in accordance with the Dartford Plan 2024 Policy M3.

Compliance

30. **Street Light Dimming** – The street lighting hereby approved, both adopted street lighting and private street lighting, shall follow the Kent County Council Highway's dimming regime and be dimmed by 40% between the hours of 20:00 to 22:00 and then dimmed by minus 60% between the hours of 22:00 to 05:00 every day of the week.

Reason: In the interests of ecological and biodiversity protection in accordance with the Dartford Plan 2024 Policy M14.

31. **M4(2)** - All dwellings and apartments hereby approved, with the exception of the Flat Over Garage (FOG) units, shall be designed and constructed in accordance with Building Regulations Part M4 (2) - Accessible and Adaptable Dwellings.

Reason: To ensure that the internal layout of the building provides flexibility for the accessibility of future occupiers and their changing needs over time, in accordance with the Dartford Plan 2024 Policy M8.

INFORMATIVES:

1. **ADDITIONAL DOCUMENTS**

The following documents have been considered in the assessment of this planning application:

- P21-2399_EN_0003_S1_REV C – Street Furniture Palette
- P21-2399_EN_0002_S1_REV C – Boundary Treatments
- Balcony Sizes Rev 2 (dated 12.02.2025)
- Car Park Management Plan (Ref: 173534-05A, dated December 2024)
- Design and Access Statement (dated 19th September 2024)
- Design and Access Addendum dated 10th January 2025
- Overheating Risk Assessment (OCSC, ref C1037/1, dated 02 January 2025)
- Sunlight and Daylight Report (eb7, Ref:6691, dated 20 December 2024)
- Response to LLFA (Ardent, ref: CG/173534/Letter, dated 8 November 2024)
- Residential Travel Plan (Ardent, Ref: 173531-03C, dated December 2024)
- Noise Assessment (Ardent, Ref: 173534-01A, dated September 2024)
- Noise Assessment letter (Ardent, Ref: LD/173534-08/Noise, dated 18 December 2024)
- Noise Assessment Addendum (Ardent, Ref: 173534-R09A, dated January 2025)
- Drainage Technical Note (Ardent, Ref: 173534-04G, dated December 2024)
- Highways Technical Note Addendum (Ardent, Ref: 173534-07, dated December 2024)
- Revised Ardent Response to KCC Highway comments dated 6 February 2025
- Construction Environmental Management Plan (Latimer/Countryside Partnerships)
- Ecological Walkover (Middlemarch, ref: RT-MME-181127, dated 18 September 2024)
- Planning Statement (Savills, September 2024)
- Community Participation Statement (Savills, September 2024)
- Cover Letter (Savills, 19 December 2024)
- Energy and Sustainability Statement Sept 2024 (AES, Rev 5 dated 10.01.2025)

2. **OUTLINE PLANNING PERMISSION**

The applicant is reminded that this decision is an approval of reserved matters pursuant to an outline planning permission and that the conditions attached to the outline planning permission also have to be complied with.

3. **EMPLOYMENT ACTION PLAN**

The applicant is reminded that an Employment Action Plan is secured under the S106 of the outline permission which requires the Owner to use reasonable endeavours to implement a plan from implementation to completion of the development to encourage the employment of local residents and local companies as a contribution to the regeneration of the local economy.

4. DEVIATION FROM THE APPROVED PLANS

It is possible that any proposed deviation from the approved plans could be classed as a 'material' change requiring a further application/permission. In the event that any change is proposed, applicants are advised to seek advice from the Local Planning Authority as proceeding without the necessary permissions could nullify this approval.

5. BUILDING REGULATIONS AND OTHER CONSENTS

This decision DOES NOT imply any consent, which may be required under the Building Regulations or under any other enactment or provision. Nor does it override any private rights which any person may have relating to the land affected by this decision, including the provisions of the Party Wall etc. Act 1996.

6. NAMING AND NUMBERING

Your attention is drawn to the need to contact Dartford Borough Council's Street Naming and Numbering Officer in order to have the new properties formally addressed.

7. KENT COUNTY COUNCIL HIGHWAYS AND TRANSPORTATION

It is the responsibility of the applicant to ensure, before the development hereby approved is commenced, that all necessary highway approvals and consents where required are obtained and that the limits of highway boundary are clearly established in order to avoid any enforcement action being taken by the Highway Authority.

Across the county there are pieces of land next to private homes and gardens that do not look like roads or pavements but are actually part of the road. This is called 'highway land'. Some of this land is owned by The Kent County Council (KCC) whilst some are owned by third party owners. Irrespective of the ownership, this land may have 'highway rights' over the topsoil. Information about how to clarify the highway boundary can be found at <https://www.kent.gov.uk/roads-and-travel/what-we-look-after/highway-land/highway-boundary-enquiries>.

The applicant must also ensure that the details shown on the approved plans agree in every aspect with those approved under such legislation and common law. It is therefore important for the applicant to contact KCC Highways and Transportation to progress this aspect of the works prior to commencement on site.

Please note: Planning permission does not convey any approval for construction of the required vehicular crossing, or any other works within the highway for which a statutory licence must be obtained. Applicants should contact Kent County Council - Highways and Transportation (web: www.kent.gov.uk/roads_and_transport.aspx or telephone: 03000 418181) in order to obtain the necessary Application Pack.

8. LANDSCAPING

The applicant is reminded that all planting, seeding and turfing comprised in the approved scheme of landscaping shall be implemented in accordance with the approved details and in accordance with the timing as set out in condition 64 of outline permission EDC/17/0048.

9. VEGETATION CLEARANCE

The site has potential to support nesting birds (including skylarks and black redstart) and therefore the applicant's Ecology Report has detailed that a pre-commencement survey must be carried out if works commence within the breeding bird survey (March to September inclusive) but the season may extend depending on weather conditions. The applicant's ecologist can advise on exact timings. Works must take place in accordance with the requirements of the Wildlife and Countryside Act 1981 in relation to the protection of nesting birds.

10. STRATEGIC ACCESS MANAGEMENT AND MONITORING STRATEGY (SAMMS)

The applicant is reminded that payment of the SAMMS contribution must be paid prior to commencement of the development hereby permitted, as required by the S106 Agreement under the outline consent .

11. PERMITTED DEVELOPMENT RIGHTS

The applicant is reminded that the outline consent for Eastern Quarry removes permitted development rights for alterations to dwellings. The car ports set out in the approved plans must be maintained with no enclosure to the front and rear elevations without prior permission and future residents should be advised of this.

12. POSITIVE AND CREATIVE APPROACH TO DECISION-TAKING

In accordance with paragraph 39 of the National Planning Policy Framework, Ebbsfleet Development Corporation, as local planning authority, has approached the proposed development in a positive and creative way, focusing on finding solutions.

- Pre-application advice was given;
- The agent was advised of planning issues during the processing of the application and additional information has been submitted to address these; and
- The application was determined within the relevant timescales as agreed with the applicant through an Planning Performance Agreement.

1.0 SITE CONTEXT AND PROPOSAL

Background

- 1.1 The application site is formed of an area of brownfield land situated within the Eastern Quarry (EQ) strategic development site in Ebbsfleet.
- 1.2 EQ, which measures some 270ha in area, is bounded by the A2 to the south, the B255 and Bluewater shopping centre to the west, and Alkerden Lane and the settlements of Greenhithe and Swanscombe to the north. Outside the boundary of EQ, the Northfleet West Sub Station site (known as Ebbsfleet Green) is under construction by Redrow to the southwest.
- 1.3 Outline planning permission was granted for EQ in 2007 for up to 6,250 dwellings together with 33% associated open space and up to 231,000 square metres of built floorspace for business premises; education community & social facilities; hotels; theatre; supporting retail and leisure facilities. All matters were reserved for later consideration. The outline consent was varied in 2013 to allow for a change in land disposition across the site. A second variation to the outline approval was granted in March 2018 to allow alterations to parameter plans reflecting an update in land levels and land use disposition.
- 1.4 The masterplan for EQ takes the form of three connected villages. The first village to come forward is Castle Hill at the east of EQ. With the exception of some small parcels of land, detailed consent has been granted across Castle Hill and as of the end of January 2025 1602 homes have been completed. Castle Hill is adjoined by Alkerden central village to the west which will form the commercial and community heart of EQ and currently construction is ongoing. Ashmere is the third and westernmost village, characterised largely by residential development with a small local retail/community focus at its centre. To date 380 homes have been completed and construction is ongoing.

- 1.5 The outline permission sets a structured approach to the consideration of reserved matters as phases come forward over time. Informed by the parameter plans, a suite of high-level, site-wide strategies was secured under the original consent which provided a framework of overarching aims and principles. Drawing down from these strategy documents, the outline permission requires an area masterplan (AMP) and area design code (ADC) to be provided for each area, against which detailed consents are to be benchmarked.
- 1.6 A combined AMP/ADC for Ashmere was approved by the EDC planning committee in June 2019. It sets out very high quality and comprehensive design parameters relating to urban form, character, street design and open spaces, to help facilitate the delivery of high-quality reserved matters applications.
- 1.7 The AMP/ADC was also accompanied by a Parking Management Plan (PMP) which sets out the vision for parking in EQ. The PMP acknowledges there is a place for the car but recognises emerging trends for lower car ownership. It therefore orientates development around more sustainable forms of travel and lowering reliance on private car use by setting out appropriate car parking ratios against Initiatives such as car clubs and public transport orientated measures.
- 1.8 All applications for reserved matters are expected to fully comply with the AMP/ADC and PMP unless robust justification can be provided.
- 1.9 Reserved Matters approval was given for Ashmere Phase 1 on 21 April 2020. Phase 1 includes 281no. dwellings together with a section of the central spine road joining to the Hedge Place Roundabout and a portion of the secondary street which runs north to south from the central spine road and abuts the western boundary of the application site. Phase 1 is complete and all 281 homes are occupied. Reserved matters approval was given for Ashmere Phase 2 on 22 July 2022 for 235 dwellings and the creation of a neighbourhood green. Phase 2 is approximately 60% constructed at the time of writing with 55 private completions and 44 units handed over to the appointed Housing Association. The current forecast for completion of Phase 2 is March 2026.
- 1.10 Separately full planning permission was granted in August 2020 for the erection of a temporary sales and marketing suite with associated parking and landscaping to facilitate the sale of new homes in Ashmere Village. This was coupled with a Reserved Matters application for the construction of 2no. dwellings to be used as show homes which will eventually revert to residential use as part of the wider Ashmere Phase 2 development.

Application Site

- 1.11 The application site is in two parts, with the smaller area to the north of the Fastrack road and the larger area situated south of the Fastrack road. Overall, the site area for Phase 3 is approximately 3.94ha.
- 1.12 The AMP/ADC identifies a landscaped zone wrapping round the entire Ashmere village. The Phase 3 development site remains inset from this landscaped zone with future development phases between the site and these village edge areas. The Fastrack road runs through Phase 3, but is outside of the redline boundary envelope, and creates the northern and southern parcels. The western and southern boundaries of the site are defined by the secondary street, which continues the street permitted in Phases 1 and 2. Future phases would abut the opposite side of the secondary street to the west and south. The northern boundary of the site adjoins the approved Phase

2 development. The eastern boundary of the site adjoins a tertiary street and the northern parcel bounds the future Village Green.

- 1.13 The application site will be accessed from the north along the secondary street, and will ultimately join to the Hedge Place roundabout to the west. The secondary street would continue east before finally connecting to the southern primary street running through Ashmere.
- 1.14 Former quarrying activity at the site has created a dramatic landscape with steep chalk cliffs around the perimeter. Much of the site has been backfilled with Thanet Sand over the worked chalk but there are still considerable land level differences between the quarry floor and the site's surroundings.
- 1.15 Earthworks for Ashmere have previously been approved under a separate planning application for advance infrastructure. In accordance with the approved parameter plans, Ashmere has a more regular topography than other areas of EQ. The topography of the application site is a slight fall from north to south over approximately 5m. The dramatic chalk cliff setting is still evident despite the infill and is a prominent feature. Phase 3 is located in excess of 190m to the west and the south of these cliffs at its closest point.
- 1.16 The majority of Ashmere village was previously occupied by a very large lake which was created in association with the former quarrying activities. This lake has been drained and filled to provide a large section of the development platform for Ashmere. Phase 3 lies beyond the lake position and the approved earthworks are completed.

Proposal

- 1.17 The application seeks approval for all reserved matters (access, appearance, layout, scale and landscaping) pursuant to the extant outline permission for EQ for the construction of 216 dwellings, together with associated infrastructure including roads, surface water attenuation and an electricity sub-station as well as the proposed discharge of condition 28 (Noise Report) pursuant to outline planning permission reference EDC/17/0048.
- 1.18 The proposed development comprises a mix of apartments, including Flats Over Garages (FOGs), and houses as follows:

Dwelling Size	Apartments	Houses	TOTAL
1-bed	36	0	36
2-bed	59	29	88
3-bed	2	21	23
4-bed	0	69	69
TOTAL	97	119	216

- 1.19 The application site is broadly made up of two urban typologies: high density, four-storey apartments and three-storey townhouses adjacent to Fastrack and the Village Green, then; a medium density arrangement where three-storey housing lines the secondary street; and two-storey, mostly terraced housing, is seen in the remainder of the site. Additionally, FOGS are also utilised in mews streets to the rear of Fastrack frontage dwellings and detached dwellings are strategically placed throughout the site.

- 1.20 The application is made on behalf of Countryside Clarion (Eastern Quarry) LLP which is a joint venture between Countryside Properties and Clarion Housing Group. The application includes a proportion of affordable housing. The total number of dwellings to be delivered as affordable housing is 54 which amounts to 25% of the overall phase. Of these affordable units, 79.6% are to be shared ownership and 20.4% affordable rent in accordance with the s106. The proposed affordable housing mix is as follows:

Dwelling Size	Apartments	Houses	TOTAL
<i>Shared Ownership</i>			
1-bed	10	0	10
2-bed	22	11	33
<i>Affordable Rent</i>			
2-bed	0	5	5
3-bed	0	4	4
4-bed	0	2	2

- 1.21 Pursuant to condition 25 of the outline permission for EQ, which requires the submission of details to be provided with any Reserved Matters application, details of the following sections in relation to the condition have been provided in the submission and will be discussed in the appraisal section below:

- a) Updated area plan
- b) Landscaping
- c) Materials
- d) Street lighting and furniture
- e) Boundary treatment
- f) Surface finishes and hard landscaping
- j) Cycle Parking

- 1.22 A strategy for Public Art has been identified in the AMP/ADC for Ashmere including potential locations which lie outside this parcel. Details relating to public art as required by part i) of condition 25 is therefore not relevant to this application. In addition, details relating to part k) television receivers; and m) layout of community buildings, g) schedule of open spaces are also not relevant to this application. Details of '(h) *External lighting (not street lighting)*' have not been submitted with the application but their omission is not considered to prevent the determination of the application at this time and can be addressed by further planning conditions in co-ordination with other lighting, furniture and elevational details.

- 1.23 In accordance with condition 28 (noise in relation to a principal highway), a noise assessment has been submitted to support the reserved matters application. During the progress of the application the noise assessment has been updated to also address cumulative noise resulting from the installation of Air Source Heat Pumps for all houses and FOGs and of Mechanical Ventilation Heat Recovery (MVHR) systems proposed to mitigate overheating in a small number of units.

2.0 RELEVANT PLANNING HISTORY

High Level Permissions

- 2.1 DA/03/1134/OUT - Outline permission for a mixed development comprising up to 6,250 dwellings and up to 231,000sq metres of built floorspace for: business premises; education; community & social facilities; hotels; theatre; & supporting retail & leisure facilities & associated works - Approved 13.11.07
- 2.2 DA/12/01451/EQVAR - S73 application for the variation of conditions of DA/03/1134/OUT to reflect a revised disposition of land uses across the site - Approved 18.01.13
- 2.3 EDC/17/0048 - S73 application for the variation of conditions of DA/12/01451/EQVAR relating to parameter plans and strategies. - Approved 28.03.18

Discharge of Outline Conditions

- 2.4 EDC/18/0196 - Application for the discharge of conditions 19, 20 & 21 attached to planning permission reference no. EDC/17/0048 relating to the submission of the Area Masterplan, Area design code and Air Quality Monitoring. [Including Parking Management Plan] - Approved 19.06.19
- 2.5 Furthermore, the following details approved against conditions under the OPP:
 - Site Wide Masterplan (condition 10)
 - Waste Management Plan (condition 11)
 - Water Management Plan (condition 12)
 - Archaeology (condition 14)
 - Biodiversity Action Plan: Implementation Programme (condition 15)
 - Utilities Framework (condition 16)
 - Sustainable Construction Plan (condition 23)
 - Water Conservation Plan (condition 24)
 - Contamination (condition 34)
 - Scheme of Working and Code of Conduct (condition 37)

Other Applications

- 2.6 EDC/18/0086 - Reserved Matters Application pursuant to Conditions 2 and 18 of planning permission reference no. EDC/17/0048 relating to advanced infrastructure works in the form of land forming and earthworks within the Central and Western villages (excluding Alkerden Farm) - Approved 06.03.19
- 2.7 EDC/19/0196 - Reserved Matters Application (relating to access, appearance, landscaping, layout and scale) for 281 dwellings (Phase 1) with associated infrastructure including road and surface water attenuation, pursuant to outline planning application EDC/17/0048 - Approved 21.04.20
- 2.8 EDC/20/0013 - Erection of a temporary sales and marketing suite, with associated car parking and landscaping. - Approved 01.09.20
- 2.9 EDC/20/0126 - Reserved matters application (details relating to access, appearance, layout, landscaping and scale) for the erection of two dwellings. - Approved 18.03.21
- 2.10 EDC/21/0079 - Reserved Matters application (relating to access, appearance, landscaping, layout and scale) pursuant to condition 2 of outline planning permission EDC/17/0048 for 235 dwellings (Phase 2) and associated infrastructure including

roads and surface water attenuation together with creation of a neighbourhood Green.
- Approved 22 July 2021

- 2.11 EDC/21/0148 - Reserved Matters application (details of access, appearance, layout, scale and landscaping) pursuant to conditions 2 and 25 of outline planning permission EDC/17/0048 for the installation of the Fastrack corridor, including vehicle, pedestrian and cycle access, landscaping and associated works - Approved 30 June 2022

3.0 PUBLICITY AND REPRESENTATIONS

- 3.1 The application was publicised in the Ebbsfleet Development Corporation (EDC) weekly list.
- 3.2 No neighbour notification letters were sent to local addresses as no occupied addresses are close to the application site. The proposals were advertised on site via 4no. notices and publication in a local newspaper for the following reason(s):
- Major Development
- 3.3 Site notice expiry date: 4 November 2024
- 3.4 Press notice expiry date: 7 November 2024
- 3.5 No letter of representation were received from members of the public or councillors.

4.0 CONSULTATION RESPONSES

- 4.1 The following organisations were consulted on the application and responses received are summarised below:

KCC Highways and Transportation:

Received 13.02.2025 – NO OBJECTION SUBJECT TO CONDITIONS

Following initial comments seeking further information and raising specific concerns KCC officers have confirmed:

- Most previous comments have been suitably resolved
- The materials to be used for the overrun area to the eastern side of the southwest bend on the secondary road should be designed such that it does not encourage general use, which may enable vehicles to travel faster through widening the bend. Materials, and the associated structural design of this area, including any kerbing separating the overrun from the main carriageway, must be sufficient to withstand repeated HGV overrun. The use of block paving with suitable detailing and appropriate edge restraints is likely to provide the desired effect, with expected reduced maintenance requirements compared to “cobble” setts or similar products.
- The extended area on the western side of the southwestern bend should be of continuous black asphalt with the secondary road surface
- A condition is required to prevent garage conversion to habitable rooms
- It is recommended that additional width is provided on both sides (not only one side) of parking spaces identified for potential to convert to accessible spaces
- The parking plan does not correspond with the landscaping plan in relation to spaces and trees within the parking courts for Building D
- Double yellow lines will need adding to the secondary street, which is to be adopted as public highway

- Exact details of cycle parking for visitors by each of the apartment blocks can be managed via condition
- For terraced dwellings which rely on bins being pulled past cars on the end house driveway - denotation of on-plot parking spaces is likely to discourage indiscriminate parking which could lead to bin-route access being blocked, but for all terrace dwellings to have independent access to bin storage is still preferable
- pertaining to pavements and street furniture, lighting, signs and lines and landscape remain.
- Materials with a low whole lifecycle cost and from the KCC standard palette of materials for the adopted highway should be selected for use on this site
- The boundary between publicly and privately maintainable areas must be clearly delineated on site
- From the supplied plans there are no foreseen conflicts between the proposed street tree and lighting column placements.
- The proposed luminaire and lighting class selection is acceptable
- Any vegetation that falls within pedestrian or vehicle visibility splays must not exceed 0.55 metres in width or 0.6 metres in height above carriageway level
- The street tree species proposed are acceptable. The girth of trees (measured at a height of 1.5 metres above ground level) located along the secondary street and at key junctions should not exceed 0.2 – 0.25 metres
- Suggested conditions related to: provision and protection of vehicle and pedestrian visibility splays; provision and retention of parking spaces, car ports and garages; provision and retention of cycle parking – residential and visitor; EV cycle charging facilities in communal cycle stores; EV car charging as per Highway Technical Note (Dec 2024); all EV charging at Mode 3 standard and wifi enabled; Submission of a Construction Management Plan; Full Travel Plan; further Car Park Management Plan; no garage or carport conversion; monitoring of tree girth size

EDC Officer Comment: These comments are discussed within the appraisal section. Suitably worded planning conditions and informatives are also proposed to address the matters raised and this advice has been shared with the applicant.

4.2 KCC Lead Local Flood Authority

Received 30.01.2025 – NO OBJECTION

Advise that have no concerns in relation to the proposed drainage.

EDC Officer Comment: This advice has been shared with the applicant.

4.3 KCC Ecology

Received 04.02.2025 – NO OBJECTION SUBJECT TO CONDITIONS

- A pre-commencement breeding bird survey must be carried out if works are to take place within breeding season as part of the construction management plan
- The ecological enhancement plan must be complied with and features incorporated into buildings at the time of construction
- Best practise would ensure that street lighting (adopted and private) would follow a dimming regime overnight.
- Additionally, the approved Biodiversity Action Plan (BAP) requires green roofs for the apartment blocks. Native Sedum mixes should be used to benefit biodiversity.

EDC Officer Comment: These comments are discussed within the appraisal section. Suitably worded planning conditions and informatives are proposed to address the matters raised and this advice has been shared with the applicant.

4.4 Dartford Borough Council (DBC)

Received 28.01.2025 – NO OBJECTION

The amended proposed mix including 2no. 4-bed houses is now acceptable for this development.

EDC Officer Comment: This advice has been shared with the applicant.

4.5 Southern Water

Received 18.10.2024 – NO OBJECTION

EDC Officer Comment: This advice has been shared with the applicant.

4.6 Kent Police Designing Out Crime Officer (DOCO)

Received 08.10.2024 – DESIGN RECOMMENDATIONS RAISED

The DOCO has not objected or provided specific comment but has supplied advice for the applicant with respect to general Secure by Design principles.

EDC Officer Comment: This advice has been shared with the applicant.

4.7 Kent Fire and Rescue

Received 23.01.2025 – NO OBJECTION

The Kent Fire and Rescue Service officer is satisfied with respect to emergency access requirements around the site and reminds the applicant of requirements under Building Regulations.

EDC Officer Comment: This advice has been shared with the applicant.

4.8 EDC Noise Advisor – Bureau Veritas (BV Noise)

Received 22.01.2025 - NO OBJECTION SUBJECT TO CONDITIONS

In respect to road traffic noise and cumulative noise from Air Source Heat Pumps (ASHP) and Mechanical Heat Ventilation Recovery system (MHVR). Most of the balconies have noise levels below the 55dB limit but a small number of balconies exceed this limit. It is also acknowledged that these residents can access a quieter public amenity space to offset the adverse impacts of traffic noise. Mitigation (glazing and passive ventilation) for habitable rooms in respect to road noise is required throughout the development and can be secured by planning condition. No specific mitigation is required for noise from ASHP/MHVR as the cumulative 'worst case scenario' example in the NIA is acceptable.

EDC Officer Comment: Suitably worded planning conditions and informatives are proposed to address the matters raised and this advice has been shared with the applicant.

4.9 EDC Lighting Advisor – Bureau Veritas (BV Lighting)

Received 21.02.2025 - NO OBJECTION SUBJECT TO CONDITIONS

The contour lines show the lux levels expected at the facades of residential properties is around 1 lux. It is accepted that the design has incorporated best practice measures to limit any environmental impact in terms of lighting pollution or energy usage. Lighting for Phase 3 is generally consistent with the Phase 2 proposals for street lighting however car parking lighting for the apartment blocks was proposed and agreed within Phase 2 but are not detailed within Phase 3. This can be secured by planning condition.

EDC Officer Comment: Suitably worded planning conditions and informatives are proposed to address the matters raised and this advice has been shared with the applicant.

- 4.10 UK Power Networks
Received 15.10.2024 – NO OBJECTION
UKPN has no objections to the works.
- 4.11 The following organisations and local ward councillors were consulted on the application but provided no comments – Cllr Victoria Akintomide-Akinwamide, Cllr - Jonathon S Hawkes, Cllr Danny Nicklen and, Bean Residents Association.

5.0 PLANNING POLICY

- 5.1 National Policy & Guidance
National Planning Policy Framework 2024 (NPPF)
Planning Practice Guidance (PPG)
National Design Guide
Building for a Healthy Life 2020

5.2 Development Plan

The Dartford Plan, Dartford Borough's Local Plan to 2037 (Adopted 2024):

Policy S1 - Borough Spatial Strategy
Policy S2 - Infrastructure Planning Strategy
Policy S3 - Climate Change Strategy
Policy S4 - Borough Development Levels
Policy E1 - Ebbsfleet and Swanscombe Strategy
Policy E2 - Ebbsfleet Garden City Development Principles
Policy E5 - Alkerden and Ashmere Allocation
Policy M1 - Good Design for Dartford
Policy M2 - Environmental and Amenity Protection
Policy M3 - Sustainable Technology, Construction and Performance
Policy M4 - Flood Risk and Riverside Design
Policy M7 - Affordable Housing
Policy M8 - Housing Mix
Policy M9 - Residential Amenity Space
Policy M10 - Extensions, New Dwellings and Garden Land
Policy M13 - Green and Blue Infrastructure and Open Space Provision
Policy M14 - Biodiversity and Landscape
Policy M15 - Travel Management
Policy M16 - Active Travel, Access and Parking

5.3 Other Guidance

EDC Implementation Framework 2017
EDC Design for Ebbsfleet
Ebbsfleet Public Realm Strategy 2019 (EPRS)
Ebbsfleet Sustainable Travel Strategy 2020 (STS)
Ashmere Area Masterplan and Design Code 2019 (AMP/ADC)
Eastern Quarry Parking Management Plan (PMP)
Dartford Borough Council 'Waste Collection & Recycling on new developments guidelines for developers'

6.0 PLANNING APPRAISAL

- 6.1 The main issues to be considered in connection with this proposal are the principle of development, each of the Reserved Matters (access, appearance, landscaping, layout

and scale), housing standards including amenity, affordable housing delivery, parking and cycle store provision, highway matters, sustainability and ecology. Intrinsic to the assessment of these issues is whether the proposal complies with local and national policy and the overarching aims of the suite of documents secured under the outline permission, notably the AMP and ADC.

Principle of Development

- 6.2 The Government is committed to significantly boosting the supply of housing and the delivery of a wide choice of high-quality homes to ensure development meets the market and affordable housing needs. Paragraph 11 of the NPPF identifies that planning decisions should apply a presumption in favour of sustainable development and paragraph 77 (NPPF 2024) endorses the provision of new homes that follow the principles of Garden Cities as an identification of quality. Policy S1 (Borough Spatial Strategy) of The Dartford Plan (DP) supports housing provision in Ebbsfleet, in accordance with policies S4 (Borough Development Levels) and S5 (Sustainable Housing Locations) and which sets out specific parameters for the creation of a chain of distinctive and individual but linked communities. Policy M8 (Housing Mix) of the DP supports housing development which provides for an appropriate range of housing stock and provision of a satisfactory quality of residential development. Delivery Theme 1 of the Ebbsfleet Implementation Framework promotes the delivery of a wide range of homes and tenures for all life stages, that meet local aspirations including the affordable housing requirements as set out in relevant local planning policies.
- 6.3 Outline planning permission has already been secured for the whole of EQ and its provisions drawn down through the suite of documents approved pursuant to the outline permission, which together form a comprehensive strategy and set of objectives for the delivery of housing in this location. These include the Site Wide Masterplan, Area Masterplan (AMP), Area Design Code (ADC), Parking Management Plan (PMP), Utilities Framework, Water Management Plan, Biodiversity Action Plan (BAP). The OPP dwelling limit for EQ is 6,250 dwellings and the AMP advises a maximum parameter of dwellings in Ashmere as 3000 dwellings.
- 6.4 Current approvals within Ashmere total 516 units within Phases 1 and 2. The present application is for 216 dwellings and would result in a total of 732 units this application is approved. This number would still be significantly below the maximum total number of dwellings indicated for Ashmere.
- 6.5 As informed by obligations under the OPP, the AMP/ADC commits to an even dispersal of affordable housing across Ashmere, with each phase to include a 25% provision. This application for residential development for 216 houses including 54 units (25%) of affordable housing complies with the broad development strategy as established in the AMP/ADC. (The s106 has more specific requirements with respect to Affordable Housing which will be considered later in this report.)
- 6.6 The principle of residential development at this site has therefore been established by the outline planning permission and the number of units within Ashmere, as a result of this application, would remain below the maximum dwelling number within the AMP. However, the reserved matters application will need to demonstrate that the detailed design of the proposals will not undermine the principles to be achieved across the Ashmere Village as a whole, as set out in the OPP and the approved strategy and design documents for Eastern Quarry and Ashmere village.

AMP/ADC Design Strategy – Ashmere Village

- 6.7 At an overarching level the AMP/ADC establishes a clear design narrative for Ashmere Village which draws inspiration from the cultural heritage of the local area and on elements of how the quarry came to be, from its beginnings as farmland, to the extraction and treatment of rock and its transportation to the docks at Northfleet and Gravesend for wider distribution. The narrative sets out broad rules and principles for massing, materiality, and hard and soft landscaping, before refining these principles into a set of character areas that respond to its location within the site and any local historical markers.
- 6.8 Building on these principles, the AMP/ADC for Ashmere sets out clear guidelines in a systematic approach to ensure placemaking is consistent with the identified density and defined narrative. In relation to density the site-wide masterplan sets strategic locations for densities across EQ. High densities are to be located along the Fastrack corridor and at key gateway locations and around the Village Green. The AMP/ADC details street characters and also identifies appropriate block types, frontage characters (in relation to building line), building typologies and parking typologies. They set out which combination of these elements is appropriate for particular areas to achieve the desired character.
- 6.9 The AMP/ADC for the 'Urban Ashmere' character area, in which Phase 3 sits, identifies appropriate massing, roof forms, and materials as inspired by Kent's historic town centres and their range of building types, materials, form and character, which are as a result of evolution of those town centres over time. The AMP/ADC states hard surface treatments should be predominant over extensive tree planting to achieve a tight urban grain. Whilst the application site is located within the 'Urban Ashmere' character area it meets the 'Farmland Heritage' character area to the west and the Village Green to the northeast.





(Source: Design and Access Statement – page 22)

- 6.10 In summary, whilst the proposal is considered to be acceptable in principle, the details of the reserved matters application will need to demonstrate that the detailed design of the proposal will suitably address the principles to be achieved across the Ashmere Village as a whole, as set out in the approved strategy and design documents for Ashmere Village as well as requirements of the OPP and the S106 agreement.

Reserved Matters – Access

- 6.11 Matters relating to ‘Access’ include consideration of the accessibility to and within the site for motorised vehicles, cycles and pedestrians in terms of the positioning and treatment of access and circulation routes and how these connect to the surrounding highway network.
- 6.12 Section 9 of the NPPF (2024) provides guidance for promoting sustainable transport where paragraph 109 supports opportunities to promote walking, cycling and public transport use and this is supported by Policy M15 (Travel Management) of the DP which encourages mixed use development and close interrelationship between different land uses as well as the provision of Fastrack to minimise car use and make effective and sustainable use of the transport network, while Policy M16 (Active Travel, Access and Parking) of the DP requires development to be appropriately located to minimise and manage arising transport impacts and promote the prioritisation of walking, cycling and public transport through clear linkages and permeability.
- 6.13 Relevant to all Reserved Matters, Policy E2 (Ebbsfleet Garden City Development Principles) of the DP states that development should comply with the principles set out in the Ebbsfleet Implementation Framework, the Design for Ebbsfleet Guide, the Ebbsfleet Public Realm Strategy and the Sustainable Travel Strategy. It also requires that development should *‘demonstrate how the location and layout contributes to the scale, legibility and walkability of the Garden City and contribute and connect to the street hierarchy...and enable walkability and cycling..’* as well as providing *‘accessible and attractive...local centres, and community and public facilities in walkable, vibrant and sociable neighbourhoods’*. Delivery Theme 3 in the Ebbsfleet Implementation Framework on ‘Connected People and Places’ promotes legible networks and the creation of safe, integrated and accessible transport systems, with walking, cycling and public transport systems designed to be the most attractive form of local transport. Delivery Theme 1 of the Ebbsfleet Implementation Framework seeks to ensure that the design of new neighbourhoods follows good urban design principles to deliver, attractive, safe and efficient layouts that contribute positively toward an appropriate character for Ebbsfleet.
- 6.14 The OPP for EQ established the broad locations and principles for strategic connections and road corridors within EQ, particularly in the Transport Strategy and the site-wide masterplan. Access to the Phase 3 site has therefore already been secured by high level documents.

- 6.15 The proposed phasing for Ashmere is broadly north-west to south-east. The first phase of development, including vehicular access from the Hedge Place Roundabout, was granted planning permission in April 2020. This access provides connectivity from the wider transport network and allows occupations to come forwards whilst the remainder of the village is constructed with internal connections across the three villages.
- 6.16 Phase 3 sits to the south and east of Phase 2. Vehicle and pedestrian access for residents to Phase 3 is provided from a secondary street which runs north to south through Phases 1 and 2, crosses Fastrack and then runs along the western boundary and then southern boundaries of Phase 3. This secondary road eventually joins the primary access road, outside of the phase. The form and layout of the secondary road follows that approved within the earlier phases for continuity and operational needs. As such the primary private vehicle routes through the site connects well to the wider village network and beyond. The Fastrack road runs east-west through Phase 3 creating a smaller northern development parcel within Phase 3 and a larger parcel to the south of Fastrack. The Fastrack corridor has been approved under RM permission EDC/21/0148 and the road is to be set within a 'Linear Park' creating a significant green corridor, together with footways, cycleway and bus stops. The Fastrack development will allow strategic access for Phase 3 residents and their visitors as part of the public transport, pedestrian and cycle network through the village, EQ and beyond.
- 6.17 The secondary road is also identified in the AMP/ADC as being a 'Fastrack emergency access'. As such, the secondary road is designed with sufficient width to accommodate Fastrack buses for example when the tunnel to Bluewater is temporarily closed for maintenance.
- 6.18 Key pedestrian access linkages are set out in the AMP/ADC to provide green connections through the development parcel, generally in an east to west direction. In addition to the Fastrack Linear park, the development also incorporates some no-through routes for cars with informal spaces and 'play on the way' to encourage cycle and pedestrian permeability, particularly from and to adjacent phases, in accordance with the AMP/ADC.



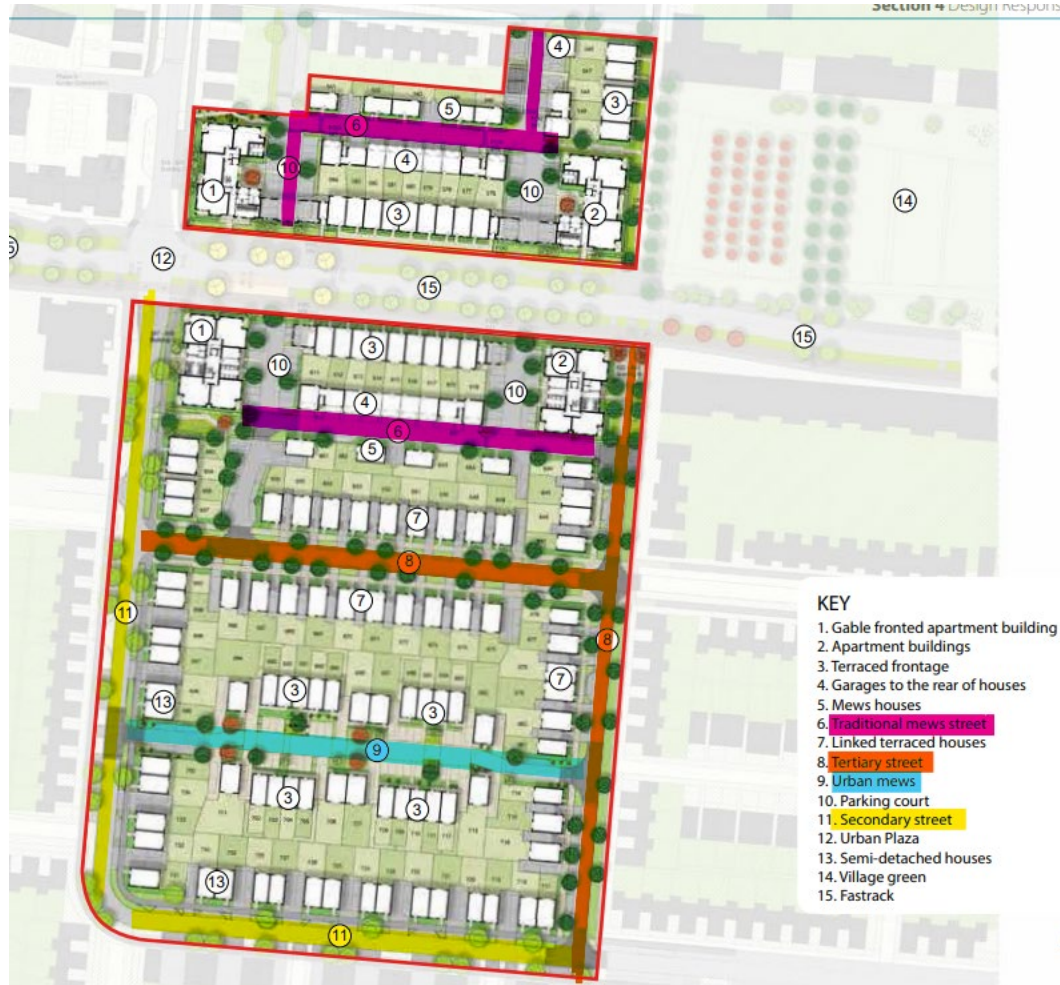
Nodal Space to Apartment Block C

(Source : Design and Access Statement – page 70)

- 6.19 Overall, the proposal demonstrates that Access to and from the site for all forms of traffic – private vehicle, cycle, pedestrian and public transport, is well integrated with the wider highway network, and routes are clear, legible and follow the principles set out within the AMPADC and as established by earlier phases.

Reserved matters - Layout

- 6.20 The matter of 'Layout' includes consideration of the way in which internal routes, buildings and open spaces within the development are provided, situated and orientated in relation to each other and to buildings and spaces outside the development.
- 6.21 In relation to the matter of 'Layout' the provision of a successful layout is intrinsic to good design which is at the heart of the NPPF core planning principles (paragraph 131). Policy M1 (Good Design for Dartford) of the DP at part c. states that good design should facilitate *'a sense of place, with social interaction, a physical environment encouraging health and wellbeing, attractive active environments and travel options... through a mix of uses and careful design and layout'*, while part d. seeks development to provide permeability through clear pedestrian and cycle linkages, active frontages, and a fine grain mix of buildings and spaces. Part 4. of the policy requires spaces to be designed to be inclusive, safe and accessible for the whole community, including young, elderly, disabled and less mobile people, with active design principles and reduce the fear of and opportunities for crime. Paragraph 117 of the NPPF encourages the provision of safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians and avoiding street clutter.
- 6.22 Policy E2 (Ebbsfleet Garden City Development Principles) of the DP requires that development should *'demonstrate how the location and layout contributes to the scale, legibility and walkability of the Garden City and contribute and connect to the street hierarchy...and enable walkability and cycling..'* and requires that development should comply with the principles set out in the Ebbsfleet Implementation Framework, the Design for Ebbsfleet Guide, the Ebbsfleet Public Realm Strategy and the Sustainable Travel Strategy.
- 6.23 At a village level the AMP/ADC identifies a clear hierarchy of streets and layout for a street network through the wider village, to ensure streets are treated as key placemaking elements which creating a different level of activation along the frontages and levels of enclosure. The location of primary, secondary and tertiary streets/mews, including areas where direct access would be permissible, are identified together with typical design layouts in the AMP/ADC. Street hierarchy and street layout within a development have a role in circulating traffic but also in creating distinct characters and successful places. Street typologies in the AMP/ADC were calculated to ensure dimensions met Kent Highway standards but also embedded strong principles of good urban design. All roads within the Phase are designed to be 20mph.
- 6.24 The Phase 3 site layout is designed to be a permeable grid layout, as endorsed by the EQ Transport Strategy, and follows a clear hierarchy and approach to the built form and narrative in accordance with the AMP/ADC. Buildings are orientated towards streets and turn corners appropriately. The site provides full permeability and a network of pedestrian-friendly routes which encourage walking and cycling through their design, layout and generous pavement widths. This includes pedestrian and cycle only routes through the site to encourage permeability and safety. Due regard has been given to secure a layout that helps avoid crime and create a safe and secure environment including natural surveillance of mews and parking areas and defensible spaces at ground floor frontages. Comments from Kent Police do not raise fundamental issues in respect of the site layout.



(Source: Design and Access Statement – Street Hierarchy – page 55)

6.25 In respect to street hierarchy the key vehicle access into the site is via the secondary road, that is a continuation from Phases 2 and 1 and runs along the western and southern boundaries of the site before exiting to the east. Branching off the secondary street are four minor streets running east to west - one identified as 'tertiary', two as 'traditional mews street' and one as 'urban mews'. Within the AMP/ADC secondary and tertiary street types incorporate highway verges, street trees and formal footpaths of each side of the street. The 'urban mews' street type provides a tighter urban grain than tertiary streets and seeks to constrain vehicular traffic, prioritising pedestrian and cycle movement. The street form utilises a shared surface with 'green pockets' of informal nodal spaces, together with offset sections of carriageway and block pavier surfacing, to reduce vehicle speeds. Within the 'green pockets' are trees, shrub planting and seating opportunities. Two streets of 'traditional mews' are located to the rear of the houses fronting onto Fastrack and allow for parking for those dwellings within garages, as well as FOGs with car ports and smaller detached dwellings which create activity and natural surveillance. These mews streets comprise shared surfaces and prioritise pedestrian movements and is the narrowest of the mews street forms. The streets are predominantly hard surfaced with thresholds in contrasting colour block banding, soft landscaping of trees, as space allows, and vertical climbers and shrub planting. Within the AMP/ADC the Fastrack corridor is also key in respect to the matter of Layout as various principles within the AMP/ADC are directed by it, such as matters of densities, building heights and primary frontages. The proposal fulfils these various principles acceptably.

- 6.26 Overall, the proposal complies with the layout and street typologies identified in the AMP/ADC.
- 6.27 With respect to technical operation of the internal road network swept path analysis drawings of various sized vehicles have been provided by the applicant, and examined by KCC Highways, which illustrate that the layout can accommodate the largest vehicles, including for refuse and fire trucks, subject to a verge overrun being provided on the inside bend of the right-angled corner at the southwestern corner of the site to allow for a refuse truck to pass a large car on the corner. In visual terms is unfortunate as it sacrifices planted verge, but on balance is accepted. A hard surface over-run is also required at the corner to the outside verge to a lesser extent. It will be required that within the adjacent Phase 4 development that the outside verge be widened on the Phase 4 side of the site boundary and the trees moved back within the verge to prevent damage by large vehicles. The applicant is in agreement with this in principle and will be reminded of this expectation by informative. While this does not directly comply with the requirements of the AMP/DC for verges within secondary streets, given the limited width verge sections created by the over-runs, the scheme is still considered to be in general compliance with the AMP/ADC, which is the test required by the OPP.
- 6.28 Refuse storage for the apartments/FOGs are integral while each house is proposed to have a storage area to the rear garden and gated entrances accommodated accordingly. It has been demonstrated refuse vehicles would be able to access the site and the drag distances for individual dwellings to their respective collection points accord with the Dartford guidelines, and in this way the layout is acceptable.
- 6.29 The gently sloping nature of the site ensures good accessibility throughout the phase and would not challenge those with mobility problems. Frontage accesses to properties are maintained where possible, including for apartments, and pedestrian access is orientated toward the street for the most part. Overall, it is considered the proposal represents a well-designed and thought-out layout and is considered to be in general accordance with the various requirements and ambitions of the AMP and ADCs in this regard.

Reserved Matter – Scale

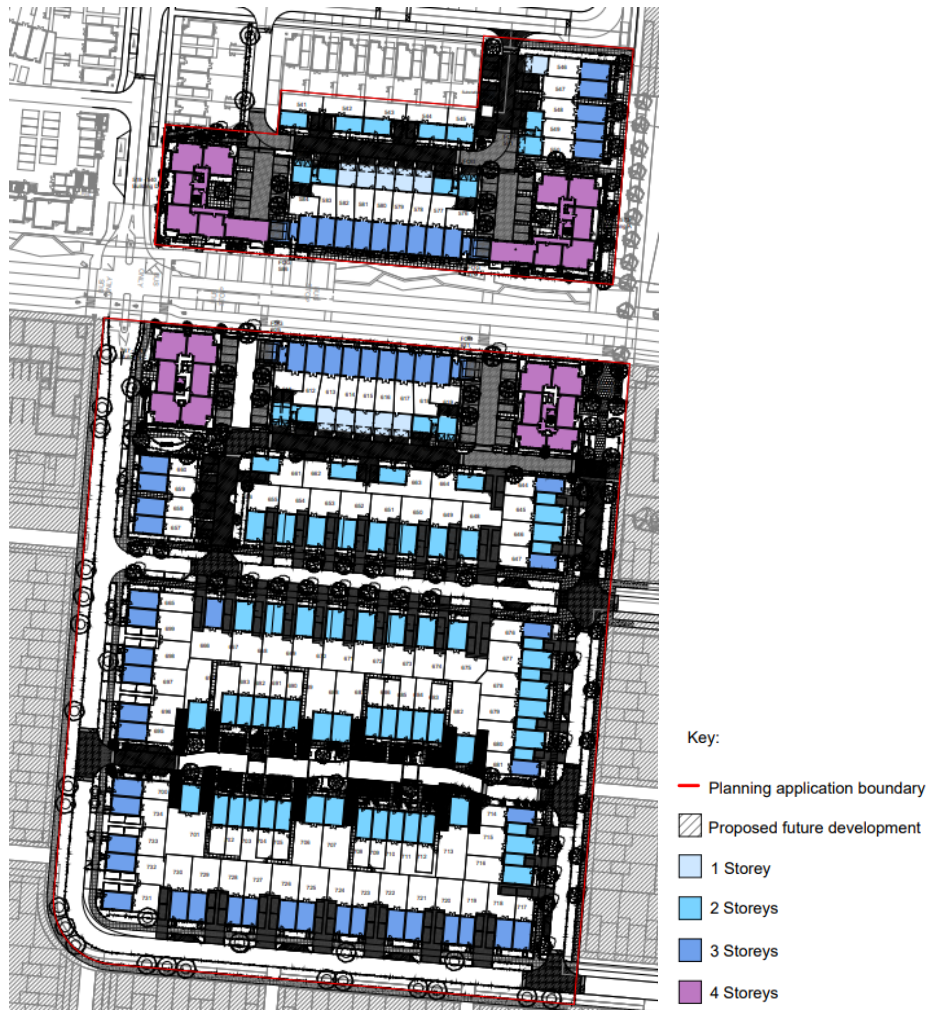
- 6.30 Consideration of scale relates to the height, width and length of each building proposed within the development in relation to its surroundings.
- 6.31 Matters of scale are intrinsically linked to good design quality. Policy M1 (Good Design for Dartford) of the DP requires developments to reinforce and enhance positive aspects of the locality and take opportunities to create distinctive high quality and beautiful places. Part 2 of the policy requires new development must be suitable in terms of its height, mass, form, scale, orientation, siting, access, overlooking, overshadowing, articulation, detailing, roof form, and landscaping relative to neighbouring buildings and the wider locality. Part 3 of the policy further provides that the scale and density of development should be the outcome of a design-led process which should have proper regard to [in part] the principle of securing a mix of uses and residential types, achieving efficient re-use of land where appropriate, and delivering regeneration at urban locations well-served by public transport and services.
- 6.32 The application seeks approval for a large-scale residential development. The densities established by the AMP/ADC sets the majority of Phase 3 at medium density (35-60 dph) with high density for the whole of the northern parcel and the northmost part of the southern parcel (60+ dph). The development proposes 216 dwellings with the sub area densities being 35.1dph and 75.26dph respectively. The overall density

for the development would be 54.96dph. These densities comply with the overall density strategy established in the AMP/ADC.



(Source : AMP – Density and Storey Height parameters – pages 65 and 67)

- 6.33 The AMP/ADC identifies appropriate heights for development according to their location, relationship to open spaces, road hierarchy and corresponding density. It sets a building height range of between 3-8 storeys located along the Fastrack corridor and to the north, south and west of the Village Green. The AMP also identifies the frontage to the north and south of Fastrack as being 'Primary Frontage' where buildings at key points should be used to add visual interest, which in one approach could be achieved by height. The AMP/ADC requires that the remainder of the Phase, to the south of the Fastrack frontage development, should have a height of 2-3 storeys.
- 6.34 The proposed built form comprises 4 storey apartment blocks and 3 storey houses to the Fastrack frontage and Village Green. 2 storey FOGS and houses, alongside single storey garages, are located along the east-west traditional mews streets where the road widths are generally more intimate. 3 storey primarily semi-detached dwellings face onto the secondary road around the perimeter of the phase. Internally, the east-west tertiary roads predominately comprise 2-storey dwellings with 3-storey dwellings at key junctions with side roads. Internal to the site are primarily two-storey dwellings.



(Source : Extract Storey Heights drawing)

6.35 Overall, in relation to the building heights there is seen to be a logic to the placement and particular height of the various buildings which works well. It is unfortunate that the development has not been able to deliver a greater height of building along the Fastrack corridor and to the perimeter of the Village Green to create greater enclosure of these primary frontages and that the development generally is at the lower end of the density parameter scale. However, the rationale is in broad accordance with the heights set out in the AMP/ADC and would complement the heights approved within Phase 2 and the proposed scale of the development would be effective in creating variety across this large site. Overall, the proposal is acceptable in relation to the matter of Scale.

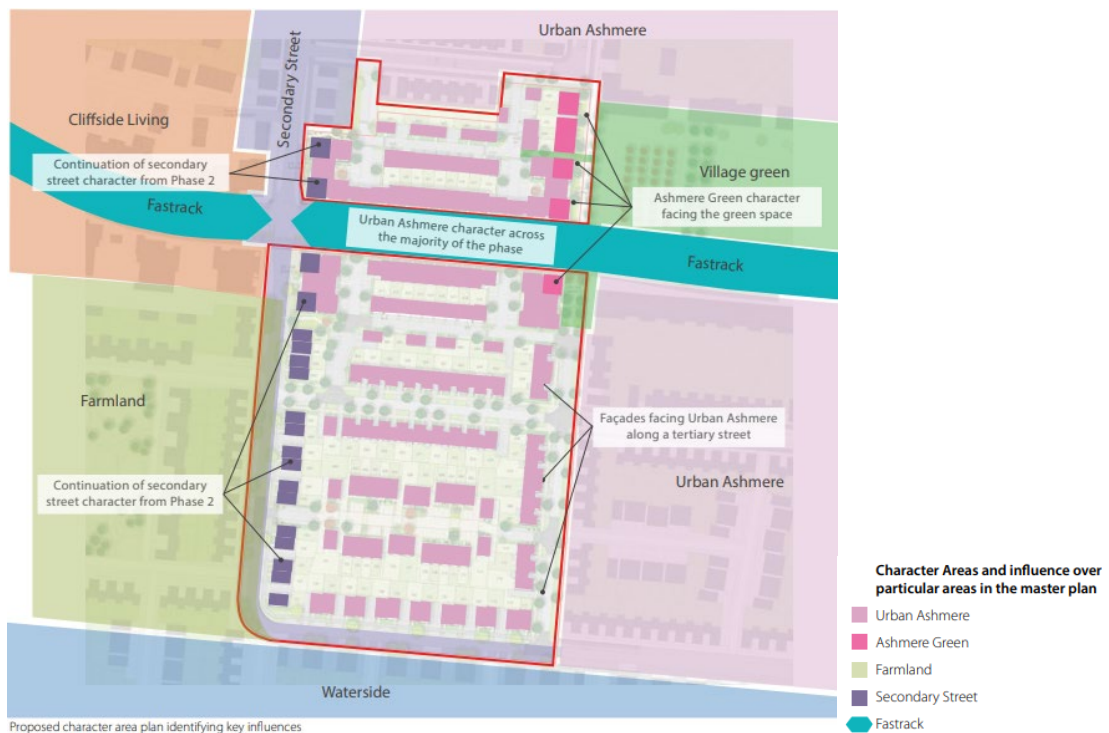
Reserved Matter - Appearance

6.36 Matters of appearance refer to a building or place within the development which determine the visual impression the building or place makes, including the external built form of the development, its architecture, materials, decoration, lighting, colour and texture.

6.37 The NPPF (2024) sets out the creation of high-quality buildings and places as being fundamental to what the planning and development process should achieve. It identifies good design as being a key aspect of sustainable development, creating better places in which to live and work and helping make development acceptable to communities (paragraph 131). Paragraph 135 states planning decisions should ensure

developments function well and add to the overall quality of the area for the lifetime of the development, be visually attractive as a result of (in part) good architecture, are sympathetic to local character and history including the surrounding built environment and states that development that is not well designed should be refused, especially where it fails to reflect local design policies...local design guidance...design guides and codes (paragraph 139). Policy M1 (Good Design for Dartford) of the DP sets out design principles for Dartford which includes reinforcing and enhancing positive aspects of the locality, with materials that support a sense of place and relate well to the local character. Policy E2 (Ebbsfleet Garden City Development Principles) requires the proposals should be of the highest quality, including beautifully and imaginatively designed homes and buildings which combine the best of town and country and exemplar developments. Delivery Theme 1 of the Ebbsfleet Implementation Framework on the creation of 'Quality Homes and Neighbourhoods' promotes the delivery of high performing, high quality, homes and buildings.

- 6.38 The architectural themes for this site have been drawn from the design narrative and corresponding built form principles set out in the AMP/ADC. The AMP/ADC identifies that Phase 3 itself falls within the 'Urban Ashmere' character area, the phase meets other character areas at it's various boundaries, and also is influenced by the established built forms along distributor level streets within earlier Phases. As such the proposed design reacts to all of these design influences and elements of transitional change between character as part of the creation of a coherent whole for the village.



- 6.39 The Urban Ashmere character area is informed by guidance set out in the AMP/ADC which identifies appropriate massing, roof forms, and materials as inspired by Kent's historic town centres and their range of building types, materials, form and character which are a result of evolution over time. The AMP/ADC states hard surface treatments should be predominant over extensive tree planting to achieve a tight urban grain.

6.40 The applicant advises that analysis of the architectural character of prominent buildings and locations within Gravesend town centre have informed the design of the apartment blocks and houses, particularly along Fastrack, and seek to generate a strong design narrative which takes the historic high street architecture in Gravesend and delivers a contemporary interpretation. There is also recognition of the transition between Phase 2 development, currently under construction, particularly along the secondary street and at the junction with the Fastrack and the 'Cliffside living' area.



(Source – Site Sections (Sheet A) drawing)

6.41 As a whole the scheme does incorporate a good mixture of housing typologies including 4 storey apartment blocks, terraces, detached and semi-detached dwellings and FOGs. Varied brick types, colours and boundary treatments have been used to achieve a tight urban grain, particularly along Fastrack which has helped to create a characterful place and a coherence of built form with the adjacent Phase 2.

6.42 The Ashmere Village Green narrative zone is located to the eastern boundary of Phase 3, adjoining the Fastrack route, before it passes the Ashmere Village Green. As outlined in the AMP, buildings around the perimeter of the Ashmere Village Green should have a distinctive identity and increase opportunities for passive surveillance onto the open space. Proposed Block A faces onto Ashmere Village Green with an elevation also onto Fastrack. The design is such that references have been made to the existing historic buildings in the area through the choice of brick colour and rhythm of fenestration, which creates a distinctive white facade. Protruding patterns of brick work along the parapet wall add a layer of visual interest along with grey brick facade articulating the entrance. Furthermore, the addition of direct access from the streets to the ground floor apartments have allowed for an increased integration with the public realm and active frontages.



(Source: Elevation drawing Block A)

6.43 Away from Fastrack, the variety of housing typologies showcase a range of brick colours and roof forms, creating a characterful area which continues the references of earlier phases. Along the secondary street and urban mews the front elevation

incorporates a glazed brick to articulate the entrance and add interest, which is considered to be an attractive feature.



(Source : Site Section (A) drawing – Secondary Streets – western boundary and southern boundary)

6.44 The tertiary streets define the eastern boundary to the Site and provide a key green route through from west to east, and north to south to Ashmere Village Green. The tertiary streets propose 2-storey terraced properties with detached 3-storey corner marker buildings. The frontage follows a linear pattern, introducing an informality through the use of asymmetrical roofscapes. The terraced dwellings are designed with a set-back carport and include articulated windowsills and larger areas of brick detailing. The 3-storey marker buildings also include an asymmetrical roof which has a shallow slope towards the adjacent 2-storey dwellings and a steeper pitch marking the corner. Brick detailing wraps around the corner and the recessed door includes coloured tiling. The proposed materials provide a consistency between the terraced and detached units with light buff facing brick proposed for all dwellings.



(Source: Site Section (B) drawing and extract Parking Plan drawing)

6.45 The urban mews provides a tighter urban grain and primarily includes terraced blocks with detached marker buildings, stepping forward of the terrace building line to create a sense of enclosure to the street. The terraced mews include a low-pitched roof and

a simple light-grey brick material palette. First floor brick detailing is proposed between the windows, with a tiled detail adjacent to the front door. The detached marker buildings have asymmetrical roofs, an off-white brick material palette and recessed entrances to contrast with the terraced dwellings and create a distinct and unique character for the area. However, front curtilage parking on both sides of the street is not compliant with the Sustainable Travel Strategy and aesthetically creates a harder street scene with a greater expanse of on-street parking, which is regrettable.



(Source: Site Section (B) drawing and extract Parking Plan drawing)

- 6.46 With respect to the traditional mews streets, which are proposed to the rear of the apartments and houses fronting Fastrack, frontages lack the same level of active surveillance as achieved in the other street typologies due to the need to accommodate garages for the Fastrack houses. The orientation of the mews houses to increase the overlooking is therefore welcomed and should help to activate the street. To enhance the quality of the street, timber garage doors have been included to add another layer of visual interest.
- 6.47 Overall, the proposal presents a high quality of building design which uses consistent elements across the scheme from other phases and within the phase itself. The applicant has responded well to the design narrative and the architectural response, including to the apartment buildings, is appropriate to the site. The range of house types is extensive with individual architectural elements such as material palette alternation, plinths, asymmetrical roofs, protruding brick patterned detailing, hit-and-miss brick panels and tiled entrances have been added sensitively and used with control to respond to the narrative for the appropriate area and different street typologies. The proposed architectural elements add distinction without being distracting or overbearing and overall, the proposal is acceptable in relation to the matter of 'Appearance'. Planning conditions can be used to agree the final materials for buildings, particulars of the architectural details and control the location and

appearance of the various domestic infrastructure that might otherwise clutter or detract from a good appearance.

Reserved Matter – Landscaping

- 6.48 This reserved matter considers the treatment of land (other than buildings) for the purpose of enhancing or protecting the amenities of the site and the area in which it is situated including screening by fences, walls or other means; the planting of trees, hedges, shrubs or grass; formation of banks, terraces or other earthworks; the laying out or provision of gardens, courts, squares, water features, sculpture or public art; and the provision of other amenity features.
- 6.49 The NPPF (2024) states that planning decisions should aim to ensure developments are visually attractive as a result of effective landscaping (paragraph 135), identifying the importance of good landscaping being intrinsic to good quality design, as advocated by policy M14 (Biodiversity and Landscape) of the DP, which states that landscaping schemes for major developments should be visually attractive, enhance biodiversity, use native species and incorporate sustainable drainage measures. Delivery Theme 4 of the Ebbsfleet Implementation Framework emphasises bringing in the 'green' landscaping to promote healthy landscapes and encourage active lifestyles.
- 6.50 The AMP/ADC identifies that the design of Ashmere village will be 'landscape-led' to ensure easy access and engagement with the natural environment to support mental and social health. Extensive detailing is set out for the design of open spaces and planting within the public realm, including consideration of verge widths to ensure an appropriate number of trees can be accommodated in the street. A landscape narrative accompanies the design narrative setting out high quality guiding principles for each character area to ensure the approach to landscaping compliments the character of the area. Specific details include hard surface materials, planting patterns, tree sizes/species and street furniture palette.
- 6.51 The urban mews within the Urban Ashmere character area includes landscaped areas with seating for impromptu gatherings and help to create a pedestrian friendly environment and slow vehicular traffic. The streets form part of the green connections linking east to west across the site and join to pedestrian/cycle only routes, between blocks which provide opportunities for play on the way when travelling through the development. Examples of the thoughtful design of these areas, dependant on their location within the phase, can be seen in the language and features of the nodal areas. The green connection links at the west of the site, south of Block C and north of Block D, are more organic and informal in their layout and landscaping, reflective of the Farmland Heritage character of the development area adjoining at the western boundary and demonstrate the 'sensitive transition' between character areas required by the AMP.



(Source : Block C Play-on-the-Way drawing)

6.52 Whereas to the east of the site the nodal spaces demonstrate a more urbanised landscaping and detailing, with more formal layout and greater proportion of varied hard landscaping, such as of the link to the north of Block A and to the east of Block B:



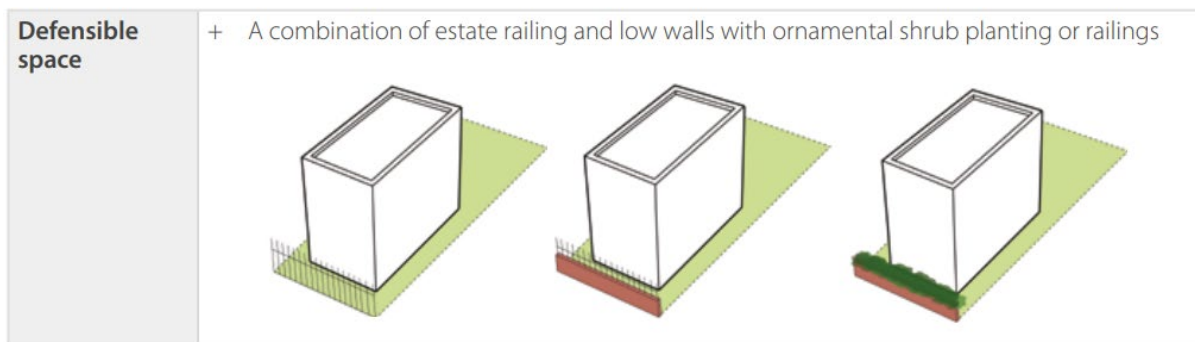
(Source : Blocks A & D Play-on-the-Way drawing – Adjacent to Block A)



(Source: DAS – East of Block B)

- 6.53 These are an example of the careful and logical application of the landscaping strategy, as an important component of the 'effective landscaping' required by the NPPF, as are the nodal spaces within the urban mews street.
- 6.54 Tree coverage is generally good across the scheme and have been located with consideration to lighting and drainage. The size of trees within the secondary street has been increased in size, over the sizes within the AMP/ADC, as Policy E2 of the DP secures development in Ebbsfleet to accord with the EDC Public Realm Strategy, which requires a larger tree size at planting for secondary streets and cars parks than in the AMP/ADC. This is a further example of part of effective landscaping being holistically planned into the development at design stage.
- 6.55 However, in relation to the western verge of the secondary street on the western side of the site, and the southern verge on the southern section of secondary street, it would have been expected that this verge would not have been within the Phase 3 envelope. When the layout of Phase 4 is known the landscaping of this verge will need to be amended as the Phase 4 development will eventually require access roads and crossovers over this verge. In discussion with the applicant, for contractual reasons the red line for Phase 3 could not be amended, but it is understood by all parties that the verge landscaping approved under this Phase will require amendment at a future date, for the purposes of a Phase 3 permission, together with potentially changes that may occur to tree placement, lighting placement and drainage. The applicant is as confident as they can be at this stage that changes will be kept to a minimum and it is reasonable to consider the application on this basis at this time.
- 6.56 Furthermore, the applicant has advised that the street lighting shown on the landscaping strategy drawings have been positioned mindful of the 5m separation required from trees and full details of landscaping specification, lighting and the routing of EV charging runs would be required by condition to ensure the infrastructure is appropriately located in relation to the specific layout and size of trees.

- 6.57 Defensible spaces are mostly clearly demarcated to support private amenity and sense of ownership. An area which needs further review and amendment is the frontage boundaries of houses fronting Fastrack and Ashmere Village Green – a level 1 street and public open space. These are required to be changed to low walls with railings, in accordance with the AMP but are currently only proposed in relation to the apartment blocks. This can be required by planning condition.



(Source: Ashmere AMP/DC – P352)

- 6.58 The various other boundary treatments through the development will, overall, respond appropriately to their location, with robust brick walls fronting public spaces, including when enclosing parking courts, with a hit-and-mix brick pattern to provide interest at key points. (One error on the Boundary Plan is noted that will need correcting and can be required as a part of this planning condition.) Boundary fences will separate and enclose private gardens. Walls seen within the public realm are also proposed to be softened by planting and focal trees in landscape beds will break up apartment parking courts.
- 6.59 Condition 65 of the outline permission states that landscaping schemes approved as reserved matters applications 'shall be implemented during the first planting season following completion of the relevant area of the development and shall thereafter be maintained for a period of five years. Any trees, shrubs or grassed areas which die, are diseased or vandalised within this period shall be replaced within the next planting season'. A detailed landscape management plan has been submitted with the application setting out objectives, tasks, methods and timing to cover all areas of planting, hard landscape and street furniture. The document identifies that long-term implementation of the management plan will be carried out by a landscape management company and that it will be reviewed annually to allow adjustments and revised every 5 years for more major changes such as the exclusion of work, as necessary.
- 6.60 Overall, the proposal is considered to presents an effective and high-quality landscaping scheme which accords with details as set out in the AMP/ADC.

Housing Mix including Accessibility

- 6.61 Policy M8 (Housing Mix) of the DP seeks to provide an appropriate spread of housing mix across developments. It states that the majority of dwellings on all developments should provide two or more bedrooms and major developments should include three and four bedroom homes. The policy also states that all new dwellings should meet the requirement M4(2): Category 2 – Accessible and Adaptable Dwellings. A limited

proportion of dwellings should also meet the requirement M4(3): Category 3 – Wheelchair User Dwellings on sites of 100 or more dwellings.

The proposed housing mix is shown below:

Dwelling Size	Apartments	Houses	TOTAL
1-bed	36	0	36
2-bed	59	29	88
3-bed	2	21	23
4-bed	0	69	69
TOTAL	97	119	216

- 6.62 The proposal accords with the policy in terms of the majority of homes 2 or more bedrooms and the scheme includes a significant number of 3 and 4 bedroom units. The Phase proposes a high number of houses - approximately 55% houses and 45% flats, with the location of apartments responding to the route of Fastrack through the site and being adjacent to the Village Green. The AMP/ADC requires a higher density of development (60+ dph) in these locations. The proposed density would meet this high-density area requirement as well as the medium density (35-60dph) requirement in the rest of the Phase.
- 6.63 The Dartford Borough Council Housing Officer has advised specifically in relation to Affordable Rented (AR) housing and local need and the applicant has suitably responded to the officer's requests and updated the AR mix. The Registered Provider for the site, Clarion Housing, has also been involved in reviewing the Shared Ownership element of the proposed housing mix and is in agreement with the mix now proposed. These conversations are discussed further in the 'Affordable Housing' section of this report.
- 6.64 In relation to 'Accessibility' development should contribute to the accommodation requirements of residents with restricted mobility, or as they age. To this end condition 30 of the outline consent states that, unless otherwise agreed in writing, 25% of all dwellings on the site shall meet the Building Regulations M4(2), 'accessible, adaptable dwellings' requirements. The AMP/ADC makes a commitment in exceedance of the outline condition that a minimum 90% of homes will be accessible and adaptable to M4(2) and above with each phase aiming to deliver this minimum target where possible and exact provision of M4(2) and M4(3) provision to be determined at Reserved Matters stage.
- 6.65 All of the proposed dwellings, other than the seven FOGs, meet the M4(2) requirements: Accessible and Adaptable Dwellings, which equates to 96.8% of units. The FOGS do not meet this as they are unable to provide step free access due to the undercroft parking. Previous phases have concluded that *'the considerable service charges that would apply if a lift were installed within the FOGS which contain only 1 or 2 apartments, it is considered reasonable that they are not provided to these buildings.'*
- 6.66 No M4(3) units (wheelchair user units) are proposed in this scheme. Whilst there is no requirement in the S106 Agreement to provide wheelchair user units, the AMP/ADC allows for the determination of M4(3) provision on a phase-by-phase basis and Policy M8 (Housing Mix) of the DP provides that on *sites proposed of 100 or more dwellings then a limited proportion of dwellings should meet the M4(3) requirements.*

- 6.67 In this case the S106 requirements were agreed at outline planning permission (OPP) stage and did not require M4(3) units and only 25% M4(2) units. As such the proposed provision of 96.8% M4(2) units is a greatly improved offering over the S106 requirements, and while it is disappointing that no M4(3) provision is proposed, it is acknowledged that there is no requirement for M4(3) housing under the OPP. 8no. parking spaces within the four apartment car parks have been designed to allow for a future adaption for vehicles requiring wheelchair access.
- 6.68 As such, and in the context of an otherwise good-quality scheme, the proposal is considered acceptable with regards to the provision of accessible housing, and overall the proposed housing mix is considered to be acceptable.

Affordable Housing

- 6.69 The provision of affordable homes supports Garden City principles to create mixed-tenure homes that are affordable for all.
- 6.70 Policy M7 of the DP (Affordable Housing) begins by establishing that residential development should aim for a genuine mix of housing to support inclusive neighbourhoods, with a range of rental and ownership options to meet *local needs*. Policy M7 requires 35% of all dwellings to be affordable housing when not in central Dartford. However, provision of an appropriate level of affordable housing was considered under the original outline permission and the obligation for delivery of affordable housing is set out in the S106 for a 25% on-site provision and 5% off-site contribution. Whilst the S106 requirements take precedence it is noted that the AMP/ADC identifies that the split should be 80% shared ownership and 20% affordable rent.
- 6.71 The S106 also requires the provision of 25% of each residential area coming forward under a RMA to be affordable housing, and seeks to ensure there is a proportional provision of affordable housing across Ashmere and Alkerden (combined) against occupation of the market housing (provision of 25% affordable at 25% occupation of market units; 50% affordable at 50% market units; 75% affordable at 75% market units; and 100% affordable at 80% market units).
- 6.72 RMAs have been approved for several parcels across Ashmere and Alkerden which are at varying stages of construction. Each of the approved schemes includes provision of at least 25% affordable housing, with the exception of the multiple Redrow phases in Alkerden South and those in Alkerden Market Centre whereby a strategic approach has been informally agreed and is being monitored separately for each of the two areas. Notwithstanding this, the EDC is satisfied that affordable housing is coming forward at a proportionate rate and will continue to monitor occupations that come forward on site to ensure these obligations against market occupation are being met.
- 6.73 The S106 identifies that affordable housing must achieve an Optimum Tenure Mix (OTM) of 5% affordable rent and 20% intermediate housing, which reflects the 20:80 split detailed in the AMP/ADC. The S106 also identifies that affordable housing shall come forward with a mix of house types and sizes (excluding tenure) which reflects that prevailing at the time for the market units, unless otherwise agreed between the Owner and the EDC in consultation with Dartford Borough Council. This requirement is drawn down through the AMP/DC which identifies multiple Phases for Ashmere in which the development will provide 25% affordable housing. It acknowledges that the precise locations and tenure details of the affordable housing will be agreed through the RM. As affordable housing should make up 25% of housing in each phase,

proportionate housing is calculated as 25% of each type (apartment or house) and size (no. of bedrooms) of unit.

- 6.74 This proposal provides 5% of the total dwellings within the Phase as Affordable Rent and 20% of the total dwellings in the Phase as Shared Ownership product which is compliant with the OTM set out in the S106. The proposed affordable housing mix (excluding tenure) provides a range of unit sizes:

Dwelling Size	Apartments	Houses	TOTAL
<i>Shared Ownership</i>			
1-bed	10	0	10
2-bed	22	11	33
<i>Affordable Rent</i>			
2-bed	0	5	5
3-bed	0	4	4
4-bed	0	2	2

- 6.75 While the proposed affordable housing mix does not represent a fully 'proportionate' mix compared to the market housing, the s106 builds in flexibility and the EDC has discretion to agree a flexible approach in consultation with DBC, whereby the present affordable housing pressures within the local area can be considered and factored in. EDC officers have discussed the proposed mix with both the Housing Officer at Dartford Borough Council, with respect to the Affordable Rented (AR) component, and with the nominated Housing Association, with regard to the Shared Ownership component.

- 6.76 The DBC housing officer advised that they currently have 42 families requiring 4-bedroomed units on the housing register and, although the demand is lower than 2-3 bedroomed units overall, the wait times for a 4-bedroomed dwelling is extremely high as the stock is low and there is little turnover of existing properties. As such the revision of the AR mix from the original proposal is welcomed. The original mix proposed:

- 9no. x 2-bed-3-person houses
- 2no. X 3-bed-5-person houses

- 6.77 The final mix offers approximately 55% family sized houses within the AR mix:

- 5no. x 2-bed-four-person houses
- 4no. x 3-bed-five-person houses
- 2no. 4-bed-6-person houses

- 6.78 This AR mix for the Phase 3 development is considered to reflect local need and is acceptable for this development as fulfilling the S106 requirement.

- 6.79 With respect to the Shared Ownership (SO) component of the AH the application proposes 22no. 2-bed apartments, together with 10no. one-bedroom apartments and then a near equal mix of 2-bed-3-person and 2-bed-4-person houses. Whilst no larger family units are proposed in the SO mix the Register Provider, Clarion Housing, confirms that there is a strong local demand for apartments (both 1 and 2 bed) as well as strong demand for 2-bed houses. They advise that *'Generally, the mix presented certainly meets demand in this location and this based off recent and live sales experience in the locality.'*

- 6.80 Overall, the revised Affordable Housing mix is seen to be addressing the evidenced local need for the area. The Dartford Borough Council housing officer agrees the AR mix is reflective and proportionate of current local need and the RP has agreed the same for the intermediate housing product and as such the overall housing mix for the Affordable Housing units proposed is acceptable, fulfilling the S106 requirement and in accordance with Policy M7 (Affordable Housing) of the DP as '*a range of rental and ownership options to meet local needs*'.
- 6.81 In terms of appearance, the affordable units share the same high-quality approach to design and the landscaping scheme is consistent throughout. The development will therefore appear tenure blind.

Housing Standards / Amenity

- 6.82 The NPPF (paragraph 198) states that planning decisions should ensure that development is appropriately located, taking account of likely effects of pollution on health, living conditions resulting from both noise and light pollution. Policy M1 (Good Design for Dartford) of the DP requires developments to '*be suitable in terms of its height, mass, form, scale, orientation, siting, access, overlooking, overshadowing, articulation, detailing, roof form, and landscaping relative to neighbouring buildings and the wider locality*'. Local Policy M9 (Residential Amenity Space) requires all new residential development to provide a '*preferably spacious (rear) garden*' for each house and a balcony, winter garden or garden terrace for each apartment. The pre-amble to the policy suggests approximately 10sqm private amenity space for each flat. Policy M2 (Environmental and Amenity Protection) requires that development must demonstrate that it is designed and located to not result in unacceptable material impacts, individually or cumulatively, on neighbouring uses and in part this is in regard to '*overshadowing, overlooking and privacy*'.
- 6.83 In addition, the AMP/ADC identifies a set of standards to be applied to high density living conditions which focus on: space and accessibility; natural light and ventilation; safety, innovation and security; consumer information (e.g. HBF/NHBC ratings); and other matters, such as provision of dual aspect apartments and refuse and cycle storage. The matter of overheating, which is also regulated under Building Regulations, needs also to be realised as part of the living condition review as it is integral to matters such as unit orientation, window placement and size, ventilation and noise.
- 6.84 The Nationally Described Space Standards (NDSS) were introduced by the Government in March 2015 as part of a revised approach to provide technical standards to drive the provision of quality housing throughout the country.
- 6.85 In accordance with the expectations for design quality and exacting standards required in the Garden City, the proposal is in full compliance with the NDSS. A significant number of the proposed units achieve floor areas in excess of the NDSS and can provide options for suitable workspaces to allow working from home. This includes dedicated workspace areas within bedrooms and living rooms and where practicable in the larger homes, separate study spaces have been incorporated into the layout.
- 6.86 The level of overlooking across the site is considered appropriate to the urban environment, balancing privacy and natural surveillance, with no direct overlooking between principal windows. Each house is provided with a good size garden and defensible spaces are clearly demarcated.

- 6.87 All the apartments have a balcony or terrace area (for ground floor apartments) but they do not meet the aspirational size within the pre-amble to the new Policy M9 of 10m². Balconies are more typically around 5m² to 7.3m² area with a handful of units having balconies much larger than this as part of the entrance design of the apartment blocks. The balcony sizes all meet the EDC Balcony Design Guidance in terms of size (minimum of 5m² for 1-2 persons and 1m² additional for each additional occupant) other than some of the terraces serving the ground floor units. A number of the terraces do not appear to have sufficient space to allow residents to dine outside, and yet appear to be able to be enlarged whilst retaining sufficient adjacent landscaping. Enlargement of these terrace areas can be controlled by planning condition. In terms of the design of balustrades has taken into consideration the form of balcony is acceptable and shown appropriate continuity with earlier phases.
- 6.88 The layout of the flat blocks is based around a central access point with stair core and lift for access to upper floor apartments and dual aspects have been maximised as far as possible. The ground floor apartments additionally would have their own private access through their own 'front door' which would further aid the privacy for occupiers of those units, as occupiers would not need to use a communal entrance, and improve the flexibility of use of those dwellings, as well as increasing the active frontage of the building and activity within the public realm.
- 6.89 In terms of management and maintenance of the external communal areas, it is considered this is covered under the Landscape Management Plan submitted with the application.
- 6.90 In relation to a quiet living environment for future occupiers, EDC's noise consultant has reviewed the noise report pack which addresses road noise (condition 28 of the outline planning permission) as well as cumulative mechanical noise from the use of Air Source Heat Pumps (ASHPs) and Mechanical Ventilation with Heat Recovery (MVHR). ASHPs are proposed to provide hot water and heat for every house and FOG, and hot water only for the apartments. The applicant's revised Energy and Sustainability Statement advises that the apartment blocks A, B, C and D will be fitted with MVHR also. All of these noise sources have been included in the revised Noise Impact Assessment (NIA) as cumulative noise and assuming an illustrative 'worst case' scenario.
- 6.91 The noise report identifies that due to noise from distant and local road traffic, closed windows will be required across some properties during the day. The windows are not proposed to be fixed shut which will allow future occupants the option to open windows should they wish. Although it is acknowledged the units will be impacted by noise, the report has concluded that suitable mitigation against road noise can be provided through the use of appropriate glazing and acoustic ventilation and secured by planning condition.
- 6.92 In relation to cumulative noise from ASHPs and other plant (including auxiliary ducts and vent points), operating at maximum capacity, the EDC Noise Adviser reports that the resultant cumulative noise would not exceed the upper CIEH/loA assessment criterion of 40 dB LAeq,15min at 1m from any noise sensitive façade of neighbouring residential premises. He agrees with the applicant's noise consultant that adoption of the upper threshold of 40 dB LAeq,T, instead of the preferred limit of 35 dB LAeq,T, is justified given the expected typical ambient and background sound levels at the site. He also notes that, whilst the assessment demonstrates compliance with this criterion, it was based on worst-case operation (maximum capacity) of ASHPs, which would likely occur infrequently. As such the noise, including cumulatively, related to mechanical plant is not of such a level that would cause harm to residents' quiet living

condition, however final details of the plant, including the location of the plant within each plot and the final details of the equipment used should be conditioned to ensure the Noise Assessment remains robust, as well as the visual impact of external units be controlled. No external mechanical plant will be allowed to the front of dwellings or on an elevation fronting a road.

- 6.93 An Overheating Risk Assessment (ORA) report has been undertaken by the applicant based on the revised scheme to review any overheating issues under Part O of Building Regulations. The report notes that the majority of the assessed rooms will meet thermal comfort with windows open but a small number of spaces will require boosted mechanical ventilation. Within the Sustainability and Energy Statement and the Noise Impact Assessment the installation of MVHR is proposed for all of the apartments. The submitted ORA indicates that very small number of non-apartments dwellings may also require MVHR, which have not been accounted for in the NIA. The applicant is confident that if any additional MVHR units are required following the formal Part O assessment the noise from any additional MVHR plant will not exceed the noise within the NIA which is based on 'worst case'. The cumulative mechanical plant noise condition already discussed will ensure this is met. It is also considered to be sufficiently demonstrated that the development will achieve a comfortable living condition in respect to overheating for future occupants.
- 6.94 External noise levels in gardens would be at an acceptable level as gardens are largely screened by the houses themselves which would provide a noise barrier. In addition, it is accepted that a certain level of noise is expected in urban locations.
- 6.95 Full street lighting details have been provided for both streets to be adopted and those to be retained in private ownership. These details have been reviewed by KCC Highways officers, EDC's lighting consultant and the KCC Ecologist. The lighting strategy applied is considered to provide good levels of illumination for pedestrians and cyclists, providing for a safe public realm, whilst the lux levels are reduced at dwelling frontages to less than 1 lux and thereby will not cause a nuisance light pollution to residents in their houses. The KCC Highway's dimming regime for adopted streets further reduces lighting illumination by 40% between 20:00 and 22:00 hours and then by 60% between 22:00 and 05:00 hours and in this way reduces wider and cumulative urban light pollution to the benefit of wider residents and nocturnal wildlife. The KCC Ecologist seeks for this dimming regime to be applied to the private streets also and this can be secured by planning condition.
- 6.96 However, within the parking courts for the apartment blocks no lighting details have been provided. Kent Police advise that these spaces can attract anti-social behaviour and as such providing sufficient lighting would help to make these spaces more attractive, safe, and usable, reducing risk of anti-social behaviour and reducing the likelihood of parking on the street. Earlier Phases have secured parking court lighting as part of the discharge of a wider parking condition but as street lighting is approved under this RM application a planning condition to provide parking court lighting in accordance with the relevant section of the BS5489-1:2020 'Design of road lighting - Lighting of roads and public amenity areas' can be secured by planning condition.
- 6.97 Overall, in respect to the above it is therefore not considered there would be undue harm caused that residents would not be aware of upon moving in, subject to suitably worded planning conditions.
- 6.98 In terms of wider amenity, the application site is former brownfield land that is currently part of a large re-development site, with development already being undertaken. The application site is located to the south of Ashmere Phase 2, these residential units will

therefore be the closest neighbours to the application site. Once again, the level of overlooking between the application site and the adjacent residential phase is considered to be appropriate to the urban environment with no direct overlooking between principal rooms across the phases.

- 6.99 In respect to construction noise a Code of Construction Practice has previously been secured under condition 35 of the outline permission. The developer is required to comply with this agreed document which would minimise noise and disturbance to existing residential neighbours.
- 6.100 Overall, the proposal measures well against the AMP/ADC set of high-density living standards, Local Plan policies and the NPPF and, with subject to the planning conditions discussed will afford a good living condition for future occupiers of the phase and neighbours.

Highways & Parking

- 6.101 The NPPF (2024) at paragraph 109 strongly advocates consideration of the impacts of development on transport networks and infrastructure, management and appropriate mitigation of environmental impacts arising from traffic and transport infrastructure, and promoting development in locations which are, or can, be made sustainable through limiting the need to travel and offering a genuine mix of transport modes. It requires that opportunities to promote walking, cycling and public transport use should be identified and pursued and recognises that patterns of movement, streets, parking and other transport considerations as being integral to the design of schemes, contributing to making high quality places. Paragraph 116 states that development should only be prevented or refused on highway grounds if there would be an unacceptable impact on highway safety. Policy M15 (Travel Management) of the DP endorses measures within new major development to manage transport demand including support for Fastrack. Policy M16 (Active Travel, Access and Parking) relates to promotion of walking, cycling and public transport, the requirements for transport assessments and travel plans, provision of footpaths and cycleways and off-street parking requirements including electric vehicle charging. The Ebbsfleet Implementation Framework seeks to deliver safe, integrated and accessible transport systems, with walking, cycling and public transport systems designed to be the most attractive form of local transport. And promote legible networks across Ebbsfleet to help people to connect with each other and between the new and existing communities (Delivery Theme 3 – Connected People and Places).
- 6.102 In respect of the individual phases of development in EQ, the overall impact of traffic movements has been established under the outline permission. No greater parking demand or traffic movements would be generated by this permission than assessed under the outline permission. A separate assessment/Travel plan would therefore not normally be requested but has been provided by the applicant in this instance.
- 6.103 The outline consent for EQ recognises that development of a site at this scale offers exciting and unique opportunities to establish its own strategy which maximise benefits offered by sustainable transport initiatives. It therefore sets out a requirement under the S106 Agreement for submission of a Parking Management Plan (PMP). The PMP was approved alongside the AMP/ADC and sets out a set of objectives which is based around the philosophy of managing car use and reducing convenience of the car. This is supported by masterplanning principles that have been established such as the priority location of the Fastrack corridor, convoluted vehicular routes and the creation of walkable neighbourhoods through the provision of accessible infrastructure together with good amounts of open space, and well-connected pedestrian and cycle links. The

vision for EQ is therefore a ‘public transport orientated development’ with the aim to embed the principle that the use of ‘walking, cycling and public transport to meet daily travel needs will challenge the historic default use of the car’ (PMP paragraph 2.1.2) and thereby foster a healthier lifestyle and promote community interaction. Building on this vision and based on empirical evidence and surveys of EQ, the PMP also sets out appropriate parking standards to be applied throughout the village.

- 6.104 The PMP parking standards require 1 parking space per apartment, 1 parking space per 1 or 2 bedroom dwelling, 1.5 parking spaces per 3 bedroom dwelling, 2 parking spaces per 4 bedroom dwelling and 1 visitor parking space per 10 homes.
- 6.105 However, further to the PMP, residential development in Ebbsfleet is required by Policy E2 (Ebbsfleet Garden City Development Principles) of the DP to comply with the principles set out in various documents including the Ebbsfleet Sustainable Travel Strategy (STS), which in relation to parking levels seeks a lower parking level than that secured in the PMP. For ‘Highly Accessible’ locations such as this site the parking requirement for dwellings is within the range of ‘0-0.8 spaces’ for a combination of residential and visitor parking combined. As such, a broader consideration of sustainable parking levels is required, which also needs to be mindful of progress in the delivery of the Fastrack service, which will run through this site and improve connectivity for future occupiers.
- 6.106 Currently, it is anticipated that the Fastrack service will start running through the site to Bluewater by September 2025. Additionally, there is a DRT bus service serving Ashmere and the Fastrack/pedestrian/cycle tunnel connecting Ashmere to Bluewater is anticipated for completion circa spring 2026. The applicant has advised that their build programme would hope to see the first occupants moving into an approved Phase 3 development in February 2026, after the Fastrack service is anticipated to start operation along the route to Bluewater through the site. In addition, items such as car clubs and dedicated cycle tracks would all support the more ambitious approach in the STS.
- 6.107 Parking is primarily provided in the form of on-plot tandem spaces to houses fronting roads although houses fronting Fastrack and the Village Green will be provided with garages off the mews roads to their rear. FOGs will be provided with parking under accommodation within car ports or on-street close by the unit. Terrace houses within the traditional mews street will utilise forecourt parking. Parking for the apartments is either within a parking court or very close by. Visitor parking and car club parking are on street and the electricity substation in the northern parcel has a parking space adjacent to it.
- 6.108 The total proposed parking provision for the Site is set out as follows:

Table : Parking and Cycle Provision Numbers

Type	STS Requirements – Highly Accessible Area	PMP Requirements	No. Spaces Proposed
Residential:			
1 & 2-bed flats	from 0 to 76	95	
2-bed houses	from 0 to 31	29	
3-bed houses/flats	from 0 to 28	35	
4-bed houses	from 0 to 90	138	
	Total = 0 to 225	Total = 297	Total = 285
Visitor	20% of total = 0 to 45	2 spaces per 10 homes = 43	= 27
Van	Non-applicable	1 space per 20 homes = 11	= 0
Car Club	2 per 200 - 399 units = 2	= 1	= 2

Sub Station	Non-applicable	Non-applicable	= 1
Total Parking	= 2 to 225	= 352	= 315
Cycle provision	1 space per bedroom = 557	1 space per bedroom = 557	= 557
Motorcycle	Non-applicable	= 4	= 0

- 6.109 In terms of parking numbers, all houses and FOGs have a minimum of 1no. allocated parking space with tandem spaces providing two spaces to some dwellings. In total, including secondary (tandem) parking spaces, 285no. residential parking spaces are provided on the site, together with the 27no. visitor parking spaces, 1no. parking space for the sub-station and 2no. for the car club. A total of 315no. spaces.
- 6.110 The total parking numbers proposed exceeds STS target by 89 parking spaces due to a high provision of on-plot tandem spaces for houses and as such does not wholly accord with the sustainable principles of the STS. However, in respect of the PMP parking requirement, the proposed development provides 38 parking spaces less than the PMP standards and as such the proposal does not wholly accord with the parking principles within the AMP.
- 6.111 On balance, there must be flexibility to consider if an acceptable parking situation can be achieved in accordance with sustainability principles without compromising the ambitions of the masterplan. Sustainability principles do not want to see an overprovision of parking and providing less parking than the PMP standards in a highly sustainable and connected location such as this is in accordance with the ambition of the more recent STS. The general provision for on-plot parking for two cars does result in an over-provision of residential spaces for houses against the STS requirements, however the applicant has amended the development through revision to exclude very elongated driveways, where occupiers may try and park a third vehicle in-line, and they have agreed to 'lining' car parking spaces on driveways which has been widely successful in earlier phases in directing correct parking. ('Lining' can be controlled by planning condition.) On the whole, the on-plot parking has been designed sensitively to contribute to the overall appearance of the development and less on-street parking does allow for greening and landscaping within the public realm to be maximised. Furthermore, the limited number of on-street parking opportunities will aid as a deterrent against indiscriminate parking. Overall therefore, while the parking provisions do not wholly accord with either the parking requirements of the PMP or the sustainability requirements of the STS the balanced approach presented in this proposal is considered to be acceptable.

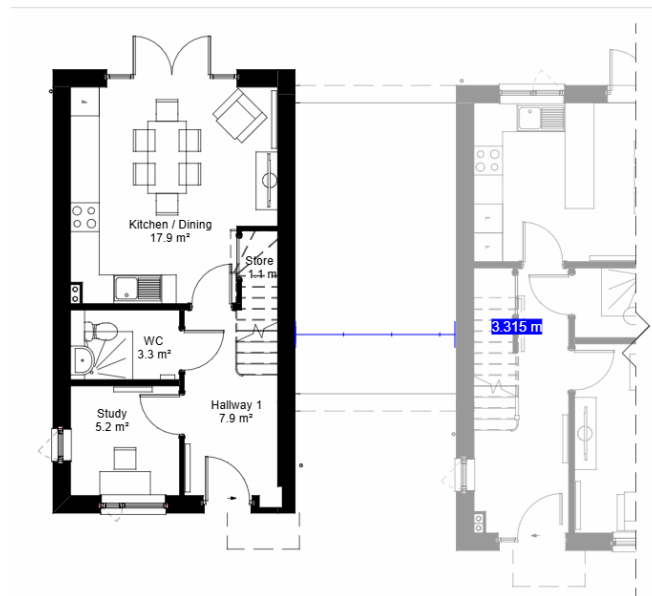
Example of the 'lining' of on-plot parking from earlier Phase:



(Source : Phase 1 site photo)

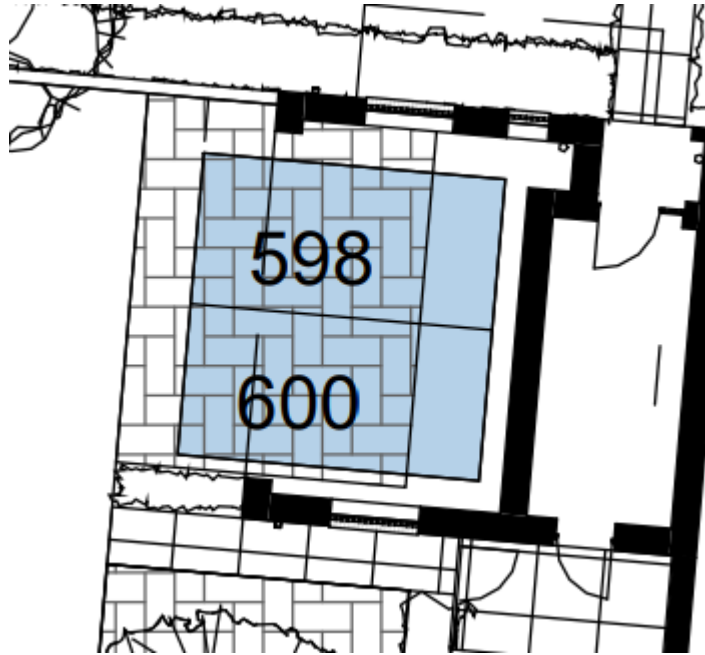
- 6.112 In relation to apartments (excluding FOGS), 67no. of the 97no. proposed apartments have an allocated parking space within one of the various parking courts. There are then 10no. 'Unallocated' parking spaces within the vicinity of the apartment blocks. The submitted Car Park Management Plan (CPMP) explains that all 2 and 3 bedroomed apartments will be allocated a parking space, but no 1-bedroomed apartments will be allocated a parking space in the first instance. The CPMP advises that occupiers of the 1-bedroomed apartments will be able to apply to lease one of the 'Unallocated' parking spaces on a flexible short-term basis if spaces are available. So, whilst the overall parking provision of the apartments is lower than the PMP of 1 space per apartment, this is considered to be appropriate in a highly sustainable area with Fastrack passing through the site, and the implication this can have on private car ownership.
- 6.113 There is no specific provision for motorcycle parking within the Site. Motorcycles would be expected to park within unallocated spaces within car parking courts for apartments, and within garages/carports or allocated parking spaces for the houses. Whilst this provision is not in accordance with the PMP, the approach is considered acceptable given the STS does not require a specific provision for residential motorcycle parking, and the overall sustainability of the Site combined with the desire to reduce the quantum of parking overall.
- 6.114 Two car club parking spaces are proposed on site, one in the northern parcel and one in the southern parcel which complies with the STS requirements and exceeds the PMP. A planning condition will need to ensure that the provision and operation of car club cars from these spaces is provided in a timely manner. The submitted Residential Travel Plan (RTP) also proposes that each dwelling will be provided with one year's free membership to the car club and £50 free driving credit, which reinforces the need for a timely provision of operational cars within the spaces.
- 6.115 The parking plan shows 27no. on-street visitor spaces are to be provided in parallel spaces spread around the site, which has been increased from 25no. spaces through redesign. While this is notably less than the PMP requirements, the provision accords with the STS visitor parking range of 0-45 street parking spaces. Moreover, due to the over-provision of tandem on-plot residential spaces, it is considered there is some flexibility for visitor parking on-plot which will absorb the under provision of on-street visitor parking spaces. Therefore, although the 27no. visitor parking spaces is not high for a development of this size, on balance this number is considered to be sufficient and is accepted.
- 6.116 The CPMP currently advises that parking enforcement would take place between 8am and 6pm with a maximum stay of 3 hours in these bays with no return within 3 hours. The Local Highway Authority (LHA) officer and EDC have advised to the applicant that stopping proactive parking enforcement at 6pm is too early and would allow for an abuse of the visitor parking spaces – for example by the parking of works vehicles or additional cars by residents – and also would encourage the parking of such vehicles in unauthorised places outside of formal parking bays unchallenged, such as bumped up on verges. In a recent residential development in Northfleet the LHA officer advised parking enforcement until 8pm would minimise the risk of residents abusing on-site parking facilities. The CPMP also states that '*Monitoring will be undertaken at regular intervals (1-2 times per day).*' The frequency and timing of visits needs also to be considered further and agreed. A revision to the CPMP can be agreed by planning condition.

6.117 In relation to car ports and garage parking, permitted development rights (PD rights) have been removed under the outline consent, and it would not be permitted to enclose the car ports without prior consent of the local planning authority or convert garages to additional habitable or incidental use spaces. The PMP states that garages are to be counted at 100% parking provision to limit visible parking on the street. There is no double-garage model shown in the PMP so the applicant has used the principles of the single garages shown in the PMP to provide a number of double garages with a shared central 1m zone for pulling bins and bikes through the garage. Car ports do not feature as a parking form within the PMP document. Where they are shown as 'driveway/undercroft', as part of a houses design, they are 3.3m width spaces in accordance with the PMP single garage model, and as already agreed and provided in Phases 1 and 2. This is appropriate for this development.



(Source : House Type 4 Type 1 drawing)

6.118 Where the car ports are related to the FOGs their design is less compliant with a 'garage' form within the PMP. In part they are of an unusual layout that they do not fully contain the car in all cases. For example:



(Source : application drawings – in this case bin storage and cycle storage are separate to the car port)

- 6.119 However, the various car ports do allow for parking spaces of the required size, although relying on partial covering of a parked vehicle only. On balance, whilst not ideal, given the small number of car ports in this form as part of the overall parking number, and that storage matters have been thought through in the designs, these are accepted as part of the overall parking provision. They are not contrary to the PMP or the STS as they do not feature specifically and do not interfere with the highway operation or harm residential amenity.
- 6.120 The PMP states *'a small number of vans were observed on site these were generally parked in residential parking spaces on plot or within visitor spaces'*. The 'small number' is set in the PMP as 5% related to housing numbers. No van spaces are provided with the Phase 3 proposal and as such the PMP requirement of 11 spaces has not been met. Whilst Phases 1 and 2 have provided van parking spaces, they have provided less than the requirement set out in the PMP. In discussion with the applicant they highlight 2no. visitor parking spaces suitable for vans are immediately adjacent to the northern boundary of the site. It is also acknowledged that it is difficult to effectively provide for vans in a residential phase as it is not known ahead of time which dwellings will have a van, and the allocation of off-plot parking spaces to units are already known, so there is little flexibility to provide for vans to specific future occupiers needs, which will also change over time as occupiers or their circumstances change. For van sized spaces as visitor spaces these can also be used by cars and as such their availability for van parking is not assured. Furthermore, the parking of work-vans overnight on the site in visitor bays is also to be discouraged by a requirement for pro-active parking enforcement until 8pm, rather than the 6pm finish within the submitted PMP, which will be required by planning condition. As such, it is accepted that no additional spaces being provided over the Phase 1 and 2 provisions. This item can be reviewed with the conditioned CPMP. Whilst justification has been requested by the KCC Highway officer no objection has been made in respect to this matter. (The applicant advises that this was agreed with KCC Highways prior to the submission of the application.)
- 6.121 Finally, in order to lower demand on car use, a Residential Travel Plan has been submitted with the application. The objective of the travel plan is to reduce the level of

single occupancy car use and particularly single occupancy journeys. It therefore identifies a series of measures for passing information on to future residents such as the availability and benefits of alternative and sustainable travel methods and benefits of homeworking. These measures are welcomed. The plan will need some modification to reflect changes to reflect the final revised development and matters related to parking management but overall it is reasonable to condition a final version of the RTP and to ensure the proposed measures are implemented.

- **Accessible Parking**

6.122 The PMP sets out guidelines for mobility impaired parking for larger, commercial/community car parks and is not explicit on residential parking. No spaces have specifically been identified in the plans as accessible parking. However, dwellings that are compliant with Building Regulations Part M4(2) should have wider spaces to allow for disabled access in the future should it be required. Moreover, the applicant advises that *'There are parking spaces in each of the [four] parking courts that have been provided with sufficient clearance to accommodate the required hatching so that they can be converted to an accessible parking space should demand arise.'* (8no. spaces in total.)

6.123 In this case it is also noted that no M4(3) units are being provided within the development. The provision for a potential future demand through the design layout of the car parks is therefore considered to be acceptable.

- **Electric Vehicle Charging**

6.124 The applicant states all parking spaces across the site are to be provided with electric vehicle charging infrastructure, including for car clubs. For unadopted roads and parking areas these will be active charging facilities from construction. Roads to be adopted by KCC will be provided with passive charging infrastructure only, which will serve the visitor parking bays within these streets. This level of provision exceeds the current requirements of Building Regulations approved Document S: Electric Vehicle Charging, but form part of the sustainability criteria for the proposal. As such the details of the charging infrastructure, including for cycle charging, can be secured by planning condition in accordance with the Ebbsfleet Sustainable Transport Strategy (STS).

- **Cycle Storage**

6.125 Cycle storage space is to be provided in accordance with the PMP and STS which is welcomed. No detailed information for ancillary storage for a cycle store has been submitted. Further details of cycle stores for houses is therefore required by condition and will seek to secure appropriate cycle storage for 1 adult bicycle per bedroom within a dwelling as per the STS requirements.

6.126 For apartments blocks, cycle storage is provided within a secure, communal cycle store with one cycle space per bedroom. 5% of these spaces have been amended through the application progression to provide a minimum of 5% provision for larger/accessible cycles. The applicant is willing to accept a planning condition with respect to securing charging for electric cycles that cannot remove their battery for recharging.

6.127 Communal visitor cycle parking facilities are shown on the updated 'Illustrative Landscape Masterplan'. The legend advises that 10no. visitor cycle spaces are proposed but the scale of the plan makes them difficult to identify. Earlier phases have ensured that visitor cycle parking location have correlated with the permeations and

green connections as set out in the AMP, in accordance with the ratios required within the PMP of EQ, and connecting with the surrounding development phases. The applicant accepts that the cycle visitor parking facilities can be addressed and secured by planning condition

- 6.128 Houses featuring double garages, which are a minimum of 7.0 m x 5.7 m and will provide space for the required storage of bicycles. For houses without garages cycle storage would be provided within dedicated sheds in the rear gardens and details of can be required and the provision secured by planning condition. All FOGs have a dedicated cycle store within the curtilage of the building. The proposal meets the PMP requirements and supports the promotion of sustainable travel in this respect.
- 6.129 The KCC Highway officer also suggests that the addition of an internal door between the entrance lobby and the cycle store within Building D will allow direct access for residents to their cycles without needing to go outside and re-enter the building. This suggestion has been relayed to the applicant. The officer also requests conditions for final details of short-term visitor cycle parking and cycle storage details for the houses. Subject to the use of a suitably worded planning conditions the proposed cycle storage is acceptable.

Sustainability

- 6.130 Section 2 of the NPPF (2024) confirms that the purpose of the planning system is to contribute to the achievement of sustainable development. Policy M3 (Sustainable Technology, Construction and Performance) of the DP seeks to preserve resources by in part by all residential development demonstrating delivery water efficiency level of not more than 110 litres per person per day, reduce/prevent waste in all development with construction materials recycled or re-used within the site or sourced locally where possible. Furthermore, through design and layout future occupiers should be able to recycle and compost. In relation to 'Energy and Carbon' all residential development should incorporate passive design in order to optimise solar and daylight access and reduce the risk of overheating and the need for cooling. Developments of more than 100 units should demonstrate significant zero and/or low carbon or sustainable technology features across the site.
- 6.131 A Sustainable Construction Code and Water Conservation Plan have been secured as strategic documents for the whole of EQ in accordance with conditions 23 and 24 of the outline permission which set out methodologies and parameters that should be implemented for all developments across EQ. The calculated water consumption for this development indicates a maximum internal water consumption of 103.8 litres/occupier/day.
- 6.132 A revised Energy and Sustainability Statement has been submitted with the application. It identifies that the development will utilise a fabric first approach to carbon energy demand together with improved insulation, efficient building services, reduced thermal bridging and air leakage paths. For the houses and FOGs efficient ASHPs for hot water demand and heating are proposed. The apartments will be fitted with ASHP for hot water demand, electric panel heating and MVHR units. No photovoltaic (PV) panels are proposed in this development although the applicant has referred to a potential future scenario where 'Future Homes Standards' (not yet confirmed) may result in plots falling under that standard being fitted with PV, which would deliver further sustainability enhancements. As this standard is not yet in place its requirements cannot be the subject of planning conditions and the proposal is assessed with no PV installation.

- 6.133 The Energy and Sustainability Statement concludes that the Phase 3 development, as designed, would significantly exceed Part L of Building Regulations which sets performance levels of 20-30% reduction in carbon emissions. The report states that *'Calculations based on a sample range of house types demonstrates that the as-designed emissions are reduced by 66.8% over Part L 2021 requirements.'* The applicant's agent has confirmed that the sample used for the calculations has included Houses, FOGS and apartments. The applicant's agent also explains that whilst the apartment heating will be provided by electric panel heaters, without any PV energy input, due to the smaller volumes and less heat loss through floors/roofs/walls of an apartment than a typical house, the energy demand for heating an apartment is significantly lower than for a house. Furthermore, MVHR would be installed in all apartments and although MVHR does use power to operate, it reduces main heating demand further by transferring the heat from outgoing air to the incoming air and this combination of fabric first approach, insulation, efficient building services, reduced thermal bridging and air leakage paths, together with the combination of domestic technologies, offers the significant carbon savings over Part L 2021. Overall, it is clearly seen from the presented calculation for the development, based on the sample, shows that the as-designed emissions reduction over Part L is significant and as such the results are acceptable. However, as these performance figures are currently based on a sample only it is appropriate to ensure that the final development performs equally well as a whole and this can be required through the use of a planning condition.
- 6.134 A Utilities Framework has been secured under the outline consent as strategic documents for the whole of EQ in accordance with condition 16. The report identifies super-fast Fibre to the Home (FTTH) will be installed to offer broadband and television services and terrestrial digital Freeview. Service providers would have the option of renting/leasing circuits on the GTC fibre network which enables the occupants the possibility to choose their own service providers since there is no exclusivity agreement. The broadband access will be gigabit capable in the interests of future-proofing and will be installed to each property prior to occupation, providing good connectivity and enabling residents to work from home, thereby reducing demand on travel.

Ecology

- 6.135 The details of ecological works, mitigation measures and monitoring have been secured under the outline permission. On-site and future works are being monitored by KCC Biodiversity Officers. The relevant ecological mitigation and site clearance has already been carried out at the site and this application relates only to the built footprint of the Ashmere village area.
- 6.136 The AMP identifies that ecological enhancements will be explored in reserved matters applications. The applicant has proposed the inclusion of 13no. Swift boxes, 3no. Redstart bird boxes, 16no. Integrated bat boxes, 10no. Integrated bee bricks and hedgehog highways spread through the phase. In addition, the approved Biodiversity Action Plan (BAP) for EQ requires that 20% of total roof areas of buildings with a roof area of greater than 250m² shall be covered with green roofs.
- 6.137 The latest revised roof plans have indicatively provided for green roofs. Final details of green roofs and the installation of the green roofs and ecology enhancement features can be secured by the use of planning conditions. The KCC Ecologist advises that native sedum mixes should be used to benefit biodiversity and the applicant has been advised of this. Planning informatives can be used to advise of the need for the applicant's ecologist to undertake a breeding bird survey prior to works commencing, if works are to take place within breeding season, and to provide best advice with

respect to the optimum placement of the ecological enhancement features on buildings and in relation to roads, open spaces and lighting.

Building for a Healthy Life (BFHL)

6.138 BFHL is a design tool to measure and assess good design for new homes that is supported by the housebuilding and architectural industries as well as Government and Local Authorities. A BFHL assessment of the scheme has been undertaken by EDC officers and results are summarised below:

BFHL Assessment

Integrating the neighbourhood

1 Natural Connections	2 Walking, cycling and public transport	3 Facilities and services	4 Homes for everyone
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Distinctive Places

5 Making the most of what's there	6 Memorable Character	7 Creating well defined streets and spaces	8 Easy to find your way around
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Streets for all

9 Healthy Streets	10 Cycle Parking and Car Parking	11 Green and Blue Infrastructure	12 Back of footpath-front of house
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6.139 The proposed development is considered to perform well against the 12 BFHL criteria. The amber scores given for Category 10 (Cycle Parking and Car Parking) relates to elements not in accordance with the Ebbsfleet Sustainable Travel Strategy and Category 12 (Back of footpath-front of house) relates to the requirement to add low walls and railings to the frontage of all dwellings fronting Fastrack and the Village Green to accord with the AMP.

6.140 Overall, the scheme is high scoring achieving 10 out of 12 greens. The results of the BFHL assessment are therefore acceptable.

Other Issues

Environmental Impact Assessment (EIA)

6.141 The outline permission was identified as being EIA development and the likely significant effects of the development on the environment were identified and assessed at the time of the outline application. If effects arising from the proposals set out in an application for approval of reserved matters were not identified or identifiable at the time of a decision to grant outline planning permission and were therefore not assessed at the time of that decision, then an assessment must be undertaken at the reserved matter stage. However, it is EDC officers' opinion that all likely significant environmental effects were identified and assessed as part of the outline permission application process and that the information before the planning authority is adequate

to assess the significant effects of the development on the environment, and therefore it is not necessary to serve a notice seeking further environmental information..

Habitats Regulation Assessment

- 6.142 Projects likely to have a significant effect upon a protected site are to be subject to an Appropriate Assessment of their implications for the Site in the context of its conservation objectives, provided that the project is not directly connected with or necessary to the management of the protected site..
- 6.143 Policy M14 (Biodiversity and Landscape) of the DP requires large residential developments located within 10km from the North Kent Special Protection Areas (SPAs) and Ramsar sites will need to undertake a Habitats Regulation Assessment to demonstrate that the mitigation measures proposed are satisfactory to avoid potential adverse recreational effects to protected features. These European Protected Sites located on the North Kent coast are the Thames Estuary and Marshes Special Protection Area (SPA) / Ramsar (wetland sites of international importance), the Medway Estuary and Marshes SPA / Ramsar, and the Swale SPA/ Ramsar sites.
- 6.144 In this instance the development is situated within 10 km 'as the crow flies' from North Kent European sites and is therefore likely to impact on the Thames Estuary and Marshes Special Protection Area (SPA). An assessment of the likely impacts from the development at EQ was undertaken at the time of the outline permission in connection with the proposed mitigation measures. The S106 in relation to Ashmere (and Alkerden) sets out a negative restriction on any development, prohibiting implementation of a planning consent without payment of a tariff based financial contribution to the Strategic Access Management and Monitoring Strategy (SAMMS) – currently £21.88 per dwelling – which pools money from local authorities to spend on measures to mitigate recreational disturbance as a result of new development. Following subsequent judicial clarification of the legal position, it is now established that the requirement for an Appropriate Assessment cannot be screened out on the basis of the mitigation measures proposed. In this circumstance, to ensure legal compliance, an Appropriate Assessment is required to be undertaken at the reserved matters stage. In the context of this application for approval of reserved matters EDC officers have carried out an Appropriate Assessment in order to assess whether any adverse impact on the integrity of the Thames Estuary and Marshes SPA can be ruled out. The Appropriate Assessment concludes that, taking into account the mitigation measures proposed in the s106 agreement, the proposal will not adversely affect the integrity of the Thames Estuary and Marshes SPA. Natural England confirmed at the time of assessing the application for OPP that a SAMMS payment (as secured by the s106 agreement) is appropriate for the development and, pursuant to the conclusions of the Appropriate Assessment in respect of this application for approval of reserved matters, EDC officers are in agreement.
- 6.145 The S106 has been completed and is enforceable against the land. A separate unilateral undertaking from the applicant for payment of the contribution is therefore unnecessary.

Archaeology

- 6.146 Condition 26 of the outline permission requires that details of foundations are provided with any detailed application to ensure that no harm is caused to any archaeological interest. Investigation of archaeological potential at EQ has already been examined in association with conditions 13 and 14 of the outline permission and the Historic Environment Framework which was approved under the outline permission. This

Framework identifies the application site is located within an area of 19th/20th century quarrying which was excavated to significant depths. It therefore concludes that there is no potential for redeposited or undisturbed Palaeolithic remains or any potential for remains of other periods. Since this has already been identified by approved documents pursuant to the outline permission and groundworks have already been carried out at the site, it is not considered necessary that foundation designs are required since no harm would arise, particularly at that depth. It is therefore considered that the application of condition 26 is not appropriate in this instance and foundation designs have not been requested.

Drainage

- 6.147 A strategy for the general handling of surface water and foul water has been secured by the Water Management Plan (EDC/19/0035) under the outline permission. Proposals for detailed design are required to accord with the strategy set out in this document.
- 6.148 A Drainage Technical Note has been submitted with the application which makes reference to the Water Management Plan. In accordance with the site wide strategy, a piped surface water network will be installed within the highways adjacent to and within the Phase 3 development. Downstream of the strategic piped network is a series of cascading swales which discharge into the Southern Lake. The surface water runoff has been assessed and sets out that all surfaces within the Phase 3 development will receive adequate treatment prior to discharge of run-off into the existing watercourses. Surface water calculations have been carried out to ensure that sufficient capacity is available based on the proposed layout. In terms of foul water drainage for the Phase 3, the development is discharged to the strategic network, within the highways, via a gravity connection.
- 6.149 KCC Lead Local Flood Authority (LLFA) have been consulted and confirm that Officers have no objections to the proposals. Liaison was had between the Applicants and KCC Drainage in December 2019 in respect of Phase 1 and the drainage design for Phases 2 and 3 has been developed in accordance with the agreed principles.
- 6.150 Southern Water raise no objection to the proposal.

Condition 25 – Outstanding Issues

- 6.151 All relevant matters listed in condition 25 of the outline permission (in relation to details required for reserved matters applications) have been addressed in the application.
- 6.152 The final detail design of some of the items, such as specific materials for the external appearance of the houses and apartments, boundary designs, hard landscaping, cycle storage, electricity meter services, external lighting and lighting columns can be secured by planning conditions.

Local Employment

- 6.153 An Employment Action Plan is secured under the S106 of the outline permission which requires the Owner to use reasonable endeavours to implement a plan from implementation to completion of the development to encourage the employment of local residents and local companies as a contribution to the regeneration of the local economy. The EDC is working with the major landowner to help implement the effective implementation of this plan in line with EDC objectives for local employment.

6.154 No Employment and Training Plan has been provided with the application but an informative will be used to remind the applicant of the S106 requirements.

7.0 FINANCIAL CONSIDERATIONS

7.1 Regard should be had to any local finance considerations in so far as material to an application for planning permission and, while this is not material to this application for the approval of reserved matters, financial benefits will accrue to the area if approval is given. The Government wishes to ensure that the decision making process for major applications is as transparent as possible, so that local communities are more aware of the financial benefits that development can bring to their area. In this area, although not considered material to this application, the following benefits to the public purse accrue from development - New Homes Bonus (which is a local finance consideration) and Council Tax for new dwellings.

8.0 HUMAN RIGHTS

8.1 The application has been considered in the light of the Human Rights Act 1998 and it is considered that the analysis of the issues in this case, as set out in this report and recommendation below, is compatible with the Act.

9.0 PUBLIC SECTOR EQUALITY DUTY

9.1 In determining this application, regard has been had to the Public Sector Equality Duty (PSED) as set down in section 149 of the Equality Act 2010, in particular with regard to the need to –

- Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under the Act;
- Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; and
- Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

9.2 The proposal has regard to the requirements of the Equality Act and has integrated various measures addressing matters of equality to minimise disadvantage, for example: the provision of a vastly greater number of dwellings achieving M4(2) accessibility level than the 25% required by the S106; permeability through to the village centre through a pedestrian and cycle network; the inclusion of tactile paving within the pedestrian network; EV charging opportunities for cycles and scooters, including visitors; car parking spaces that are designed to be able to be adapted to provide for dedicated disabled parking provision, should future demand require it, Play-on-the-Way facilities; numerous public nodal spaces that allow for informal congregation; accessible distances thought out the phase to access the Fastrack bus services.

Overall, it is considered that the application proposals would comply with the objectives of the Duty.

10.0 CONCLUSION

10.1 The proposal would deliver a high-quality residential development, including tenure-blind affordable housing, in accordance with the Area Masterplan and Design Codes for EQ and contributing to the delivery of the Garden City. The proposal is in

accordance with local and national policies and the Ebbsfleet Implementation Framework. The applicant has worked closely with the EDC to put forward a carefully considered scheme which would provide a safe and attractive living environment for future residents in keeping with Garden City principles. The application is therefore recommended for approval subject to the conditions set out in this report.